



# THE WESTON VILLAGE BIA

## STREETSCAPE MASTER PLAN 2021.02.23



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*Cover Image: Aerial of Weston Village BIA  
Source: Google Maps*

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# EXECUTIVE SUMMARY

The Weston Village BIA, in partnership with the City of Toronto, has commissioned this master plan to evaluate existing streetscape conditions, and to guide the continuing development of the streetscape. The intention is to increase the vitality of the street by identifying opportune areas for intervention.

The development of the master plan involved site research and mapping, stakeholder and client consultations, and coordination with City authorities and Councilor's Office.

The current Weston Village BIA is bound by the Humber River in a deep valley to the west and the GO/UP and CN railway tracks to the east. Weston Rd. acts as a spine through the centre of the BIA which stretches north to Church St. and south to Wilby Cres. Weston Rd. locally serves a substantial residential population in towers on the east and west sides of the street, and the single family homes of old Weston north of the tracks.

The Weston Village BIA, founded in 1979, is one of the oldest BIA's in Toronto. Surveyors first come to the area to map the Humber River before building a saw mill in Weston due to the abundance of oak and pine bush. Weston is known for the production of C.C.M. (Canadian Cycle & Motor Company Limited) bicycles from 1917 to 1983. The plant was located on Lawrence Ave. W. just east of Weston Rd. and products were distributed internationally. Penny-farthing and historic riverstone walls throughout the BIA point to its history. Today, Weston Village is known for its weekly Farmers Market (from May

to October) and the yearly Santa Claus Parade.

The north south access of Weston Rd. is largely car dominated. The change in elevation from the Humber River to Weston Rd. and inconsistent access makes it difficult for pedestrians to move easily east-west across the BIA. The Weston GO/UP Station provides easy access to the BIA from the City's core and beyond, however it currently is primarily used by residences of the area to get to and from downtown and does not promote pedestrian visitors when they arrive.

The BIA is separated into pockets of low rise retail – a typical old Toronto mainstreet mixed with large parcels including strip malls, car dealerships, low-rise residential and high rise apartment buildings in a tower-in-the-park model. The Farmers Market, Santa Claus Parade and three major parks are assets to the community and would benefit from greater visibility to attract public from outside of the neighbourhood. Weston Rd. between Wilby Cres. and Church St. is composed of restaurants, cafes, bars, personal services (including medical practices), cultural venues and residences.

The building setbacks vary frequently from residential tower developments with large green space setbacks to minimal setbacks fronting the low-rise retail pockets. Stretches of the low-rise retail offer setbacks to engage pedestrians along the sidewalk, but are only minimally taken advantage of. Impending developments are designed with retail/commercial space at the podium level, working to create a continuous

retail/commercial frontage along Weston Rd.

The City of Toronto is currently building street improvements including refurbishment of the public gathering areas at the corner of Weston Rd. and Lawrence Ave. W., sidewalk widening between Elsmere Ave. and Lawrence Ave., and additional crosswalks along Weston Rd. This road widening does not include bikeways, but lanes are slated for future discussions and implementation.

The master plan proposed in this document aims to enhance the vitality of the streetscape along Weston Rd., helping to fill gaps in commercial and pedestrian activity, and are paired with unique, site-specific interventions that target underused areas and provide greater connectivity.

The master plan incorporates the following principles:

## Enhance Connectivity

- Encourage continuity and enhance public space along Weston Rd.
- Encourage east-west access across the BIA cross streets and laneways
- Provide wayfinding and gateways for pedestrians, cyclists and cars
- Activate side streets
- Promote bike usage
- Strengthen the existing gateways through lighting, new artworks and landscape enhancements

## Highlight Humber River Adjacency

- Promote access and wayfinding to and from the river and multi use trail
- Highlight history and ecology

**TORONTO Weston Village Business Improvement Area**

- Promote a strong green agenda

Aggregate of Small Nodes

- Distribute seating across the BIA in small clusters

Vegetation Out of Harm's Way

- Maximize new tree planting/tree health
- Promote seasonal planting strategies that do not attract litter

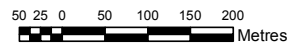
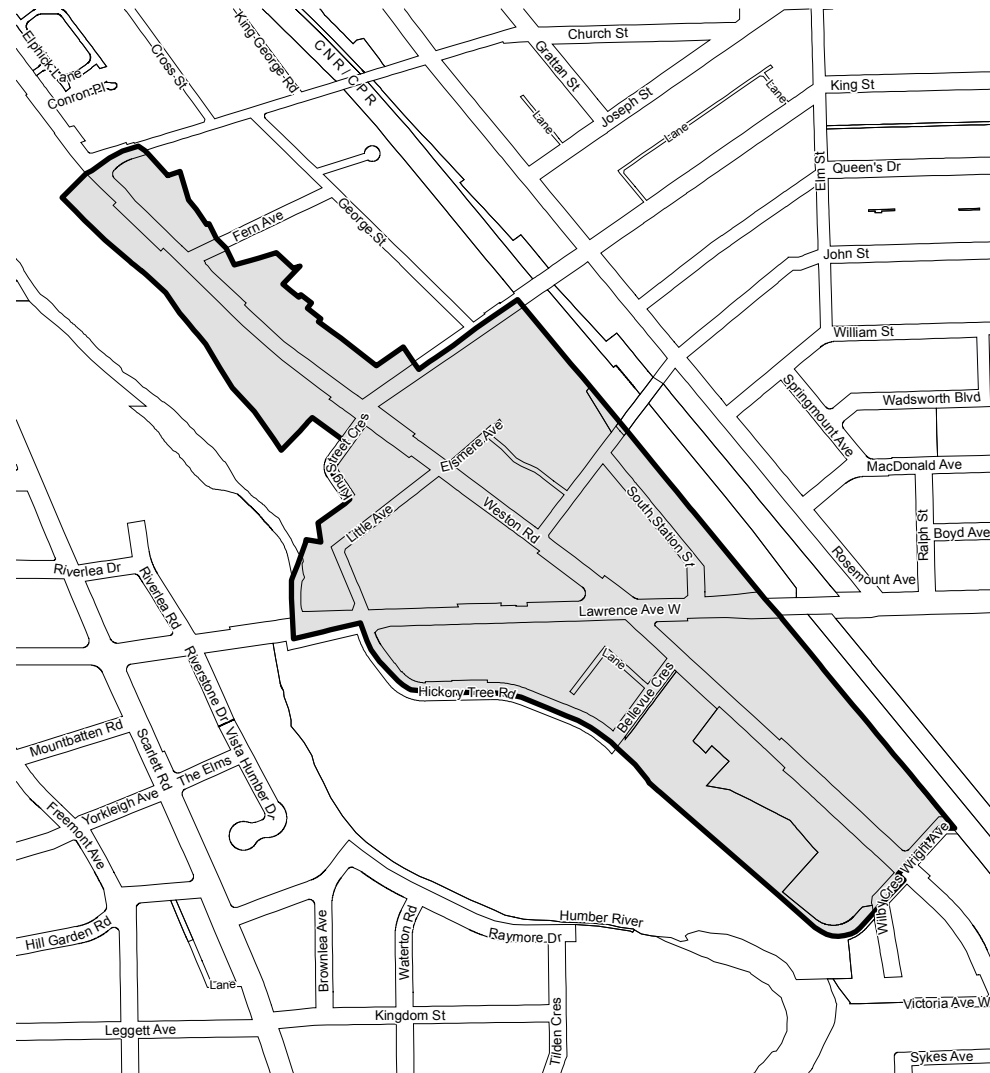
Historic Narratives

- Showcase historical narratives and artifacts
- Educate and capitalize on the historical themes of the neighbourhood: CCM industry, riverstone walls, history of indigenous peoples and the Toronto Carrying Place Trail

It Came From the Land and Weston Made it Into Something

- Honour the manufacturing past by highlighting materials and fabrication
- Celebrate the culture of bikes: promote bike culture through festivals and artwork

The master plan outlines a broad range of projects for execution over the next 5–10 years. Many interventions will involve partnerships with local building owners, small businesses and new developers throughout the BIA. Partnerships with landowners are encouraged to promote mural art, tree planting and soft landscaping along private property adjacent to the public realm.



Map File: web\WestonVillage.mxd  
Created by: GCC, February 2018.

Weston Village Business Improvement Area boundary mapping

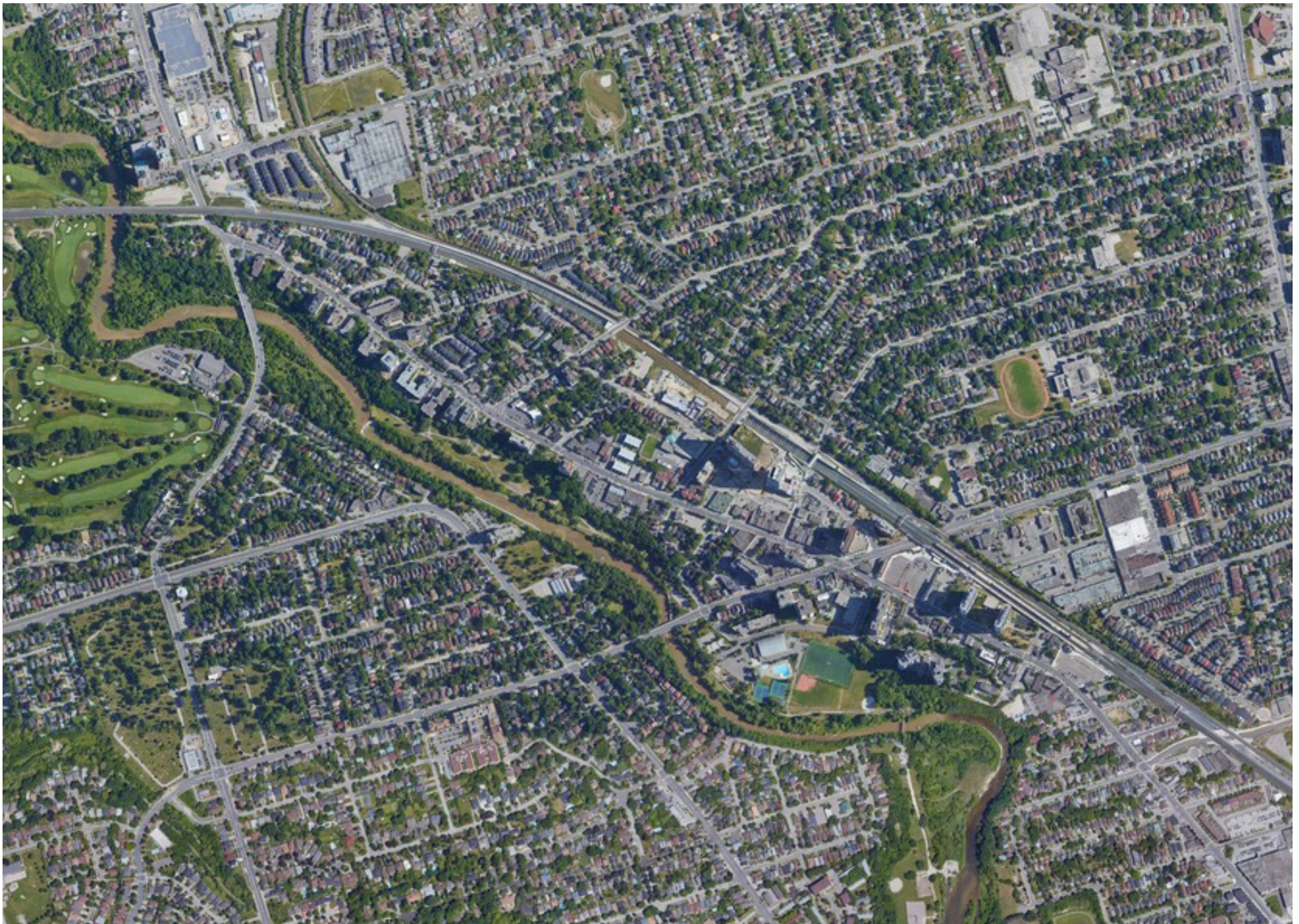
# OBJECTIVES

The Weston Village Streetscape Master Plan outlines the goals, priorities, and implementation strategies stretching from Church St. to Wilby Cres. between the Humber River and GO/UP rail tracks, that comprises the Weston Village Business Improvement Area.

The objective of the Master Plan has been to examine improvements in the public realm that can be implemented by the BIA, in partnership with the City of Toronto or other partners, over the short-term (1–3 years), mid-term (3–6 years), and long-term (5–10 years).

The primary objectives are as follows:

- Create a consistent aesthetic vision for the BIA by identifying practical, achievable streetscape improvement projects which can be implemented by the BIA in partnership with the City of Toronto;
- Enhance the retail business area to make it more attractive for existing and new businesses, while highlighting Weston Rd. as a vibrant and essential main street;
- Private property and public realm improvements to be planned in concert with redevelopment projects within the BIA. These projects should be implemented using common design elements that may be carried-throughout the BIA;
- Incorporate the ideas and priorities of the Weston Village BIA Board of Management, members of the BIA Streetscape Committee and the general membership of the BIA;
- Improve the vitality of the street and neighbourhood experience for residents and visitors;
- Promote BIA assets to increase outside visitors;
- Identify gateways and gateway design elements;
- Identify overall signage and wayfinding opportunities;
- Enhanced paving at intersections, walkways and add additional landscape treatment including furniture and planting areas.



*Aerial Image of Weston Village BIA. Source: Google Earth. August 10, 2020.*

# METHODOLOGY

PLANT Architect Inc. was retained in June 2020 by the Weston Village BIA. The limits of the study follow the outline of the current BIA borders. The intention of this master plan is to characterize a streetscape/public realm vision for the Weston Village BIA that capitalizes on its proximity to the Humber River, transportation, historic presence and retail/commercial frontage along Weston Rd.

Site analysis, including the identification of opportunities and constraints, formed the foundation for the proposed streetscape improvements. Regular meetings and feedback, costing, review of the regulatory environment, historic research, relevant planning studies, and the development context have also been used to inform the work contained in the final report.

## MEETINGS AND FEEDBACK

Over the course of the Summer and Fall 2020, PLANT met (in virtual meetings) with the BIA Streetscape Committee including members of the City of Toronto BIA staff and the Councillor's office. At the outset, we did a virtual walk of the entire site with the BIA to draw on their depth of knowledge of the Weston Village history, types of activities, aspirations and concerns. We continued to meet with the group on a regular basis to present the site analysis, synthesis of goals, master plan concepts, and final master plan and to garner feedback at each of these stages.

A public open house was held on 24 October 2020 at the Farmers Market to gather feedback from the public and the BIA General Membership. Feedback was obtained through conversation. The BIA also posted the master plan panels on their website and Instagram page. Refer to Appendix B for notes from this public engagement. In summary, there was a lot of support and excitement for streetscape improvements. Many people asked for additional support for future bikeways, and were looking for a commitment and time-line for when this would be implemented.

Top 5 comments from the public engagement:

1. The current cycling infrastructure is not sufficient, the BIA should work with City and community partners to further the development of bikeways.
2. New public art should engage and represent the community, particularly the youth of Weston Village.
3. Use vertical art elements as wayfinding landmarks and sites of commemoration.
4. Expand the sidewalks, reduce curb radii for safer turns, and add curb bump-outs where possible.
5. Add more trees and soft landscaping to the streets.

## COSTING AND PHASING

Elements of the master plan were priced by A.W. Hooker on 14 January 2021. Capital costs, associated consultant fees, required administrative processes and required interface with existing owners, builders and developers were tabulated within phases based on the priorities developed with the BIA and City of Toronto BIA partners (see Costing and Phasing section). Costing includes short term permanent, as well as long term projects requiring significant integration with City and developer work.

## REGULATORY ENVIRONMENT

PLANT reviewed the relevant regulatory standards, and guidelines to set parameters for the work:

- City of Toronto Right of Way Regulations;
- Tree Planting Solutions in Hard Boulevard Surfaces Toronto Best Practices;
- City of Toronto Urban Design Streetscape Manual;
- City of Toronto Coordinated Street Furniture Program;
- Underground Utility Plans;
- City of Toronto Construction Standards for Road Works;
- Vibrant Cities Guidelines;
- Avenues & Mid-Rise Building Guidelines;



- Toronto Sidewalk Café Manual (2015);
- Toronto Retail Design Manual (2019).

## PLANNING STUDIES AND STRATEGIES

Relevant planning initiatives specific to the neighbourhood and general urban design guidelines for the City of Toronto were considered in conjunction with this plan and relevant information is summarized in the following pages:

- T.O.INview (2020);
- Vision Zero (2020);
- City of Toronto Analysis Scores of Proposed Cycle Network (June 2019);
- Weston TAP 2.0 by Urban Land Institute Toronto (September 2018);
- Moving Forward - An Action Plan to Improve Safety and Opportunities for Pedestrians and Cyclists in Ward 11 by the Pedestrian Safety and Cycling Committee (March 2018);
- John St. Revitalization Public Realm Improvement Plan by Urban Land Institute and Dtah (January 2013);
- Weston 2021 by Urban Land Institute (February 2012); and
- Site and Area Specific Policies (45 and 51) City Planning Office Plan.

Any future work should consider the full breadth of City Planning guidelines (<https://www.toronto.ca/city-government/planning-development/official-plan-guidelines/design-guidelines/>)



*Public Meeting at the Weston Village Farmers Market*

# T.O.INVIEW (2020)

T.O.INview is the City of Toronto's web-based system that displays locations and details of past, current and planned capital work schedule for construction. Public works are programmed on a 5 year planning horizon. Phasing of projects in the BIA should consider these future public works. Relevant points are in bold. Currently, T.O.INview identifies the following projects in the area:

1. Toronto Water: trunk sewer maintenance along the Humber River Recreations Trail from Pylon Place to Tilden Cres. (2020-2023).
2. Toronto Hydro Electric: overhead Electrical at Church St. and Wright Ave. (2020).
3. Toronto Water: storm sewer replacement from Church St. to Fern Ave (2020-2021).
4. **Transportation Services and Toronto Water: major road resurfacing and sanitary sewer replacement along Weston Rd. from Church St. to St. Phillips Rd. and Lawrence Ave. W. to Church St. (2020-2021).**
5. A City policy (Moratorium) *that prevents non-emergency utility cuts from taking place in a road surface that has been resurfaced in the past five years. This is done to reduce disruption to a community where work has previously taken place and to maintain the quality of the roads.* Consult before specific projects are carried out (varies).
6. Bell: structures related to manholes, hand wells and non-linear projects on Lawrence Ave. W. (2020).
7. Parks, Forestry and Recreation: arena state of good repair rehabilitation at 2125 Lawrence Ave. W. (2019-2020).
8. Streetscape manual from Wilby Cres. to King St.
9. **Transportation Services: on-street bikeway construction on Lawrence Ave. W. from Little Ave. to Weston Rd. (2021) and from Jane St. to Weston Rd. (2021).**
10. Toronto Hydro Electric Systems Ltd.: overhead and underground civil and electrical from Lawrence Ave. W. to Wright Ave. (2020).
11. Toronto Hydro Electric System Ltd.: work in spot locations. Work may include pole installation, vault/roof rebuild, transformer changes, etc. (2021).
12. Toronto Water: sewer rehabilitation of an existing sewer pipe to extend its life and improve system performance. (2018-2019).
13. Rogers Communications Inc.: cable pulling conduit along Hickory Tree Rd. from Wilby Cres. to Belleville Cres. (2018-2019).
14. **Transportation Services: pedestrian infrastructure improvement on Weston Rd. from Lawrence to Little Ave. (2020-2021).**



**TOINVIEW Program Polygon**

■ Parks, Forestry and Recreation

**TOINVIEW Program Line**

— Bell Canada

— Enbridge Gas Distribution Inc.

— Rogers Communications Inc.

— Toronto Hydro Electric System Ltd.

— Transportation Services

— Moratorium

— Toronto Water

— Third Party Construction

— Future Moratorium

**TOINVIEW Program Point**

● Bell Canada

● Enbridge Gas Distribution Inc.

● Rogers Communications Inc.

● Toronto Hydro Electric System Ltd.

● Toronto Transit Commission

● Transportation Services

● Toronto Water

● City Planning

□ City Ward

■ TRCA Regulation Limit

■ 9999

■ Administrative Boundaries

■ Community Council Area

■ Streetscape Manual

Screen shot of the City of Toronto's T.O. Inview mapping.



SCALE: 1: 5000

Survey, Esri Japan, METI, Esri China (Hong Kong), (c) OpenStreetMap contributors, and the GIS User Community

Major Capital Infrastructure Coordination  
City of Toronto

# SITE AREA SPECIFIC POLICIES 45 AND 51

Two overall policies that provide a broad perspective of strategies within the Weston Village BIA.

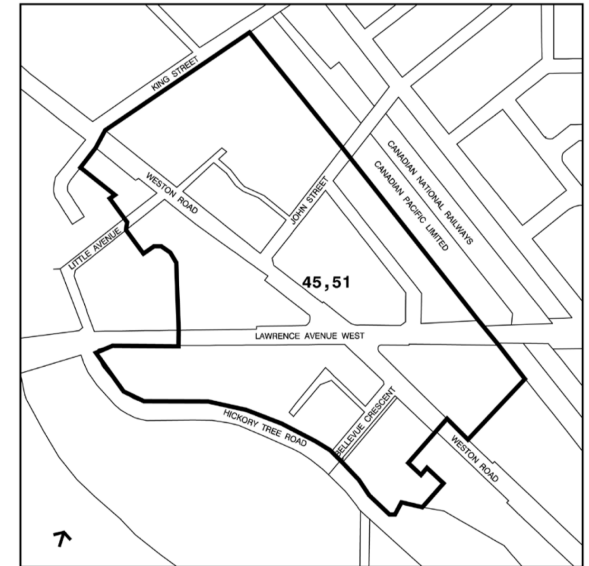
## POLICY 45: LANDS LOCATED WITHIN WESTON VILLAGE

Weston Village will be reinforced as the focus of the Weston community through implementation strategies such as:

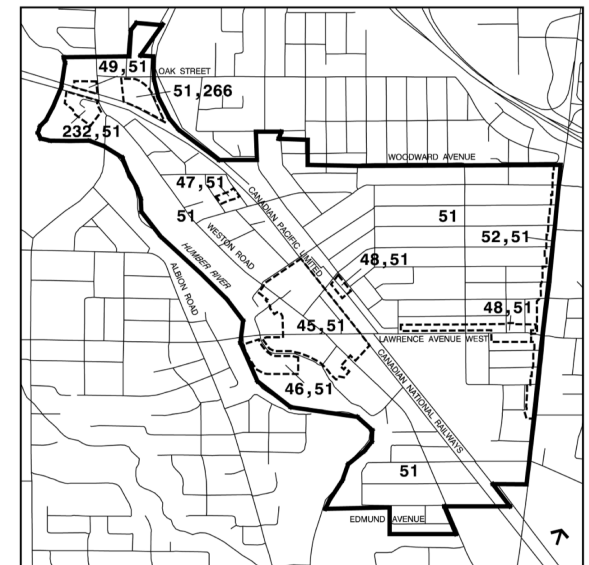
- **A streetscape design plan to improve pedestrian spaces;**
- **A façade and signage improvement program using the theme of the history of Weston Village;**
- Providing adequate public parking;
- Relocating the Farmer’s market to a site with a better connection to the surrounding communities;
- **Linking Weston Village to other parts of the Weston community and the Humber Valley through improved pedestrian connections and streetscape improvements;** and
- Improvement and expansion of the GO Station.

## POLICY 51:

- a) View corridors to the Humber Valley from street intersections of Weston Rd. in the Weston community should be maintained. **Links between the Weston community and the Humber Valley will be improved by adding and improving existing pedestrian connections** including: an expansion of the pedestrian/bicycle trail system north of Cruickshank Park and establishing additional amenities for park users such as a café, retail pavilion, and washrooms.
- b) A significant open space feature should be provided in the Lawrence employment area.



Policy 45 boundary



Policy 51 boundary

# VISION ZERO (2020)

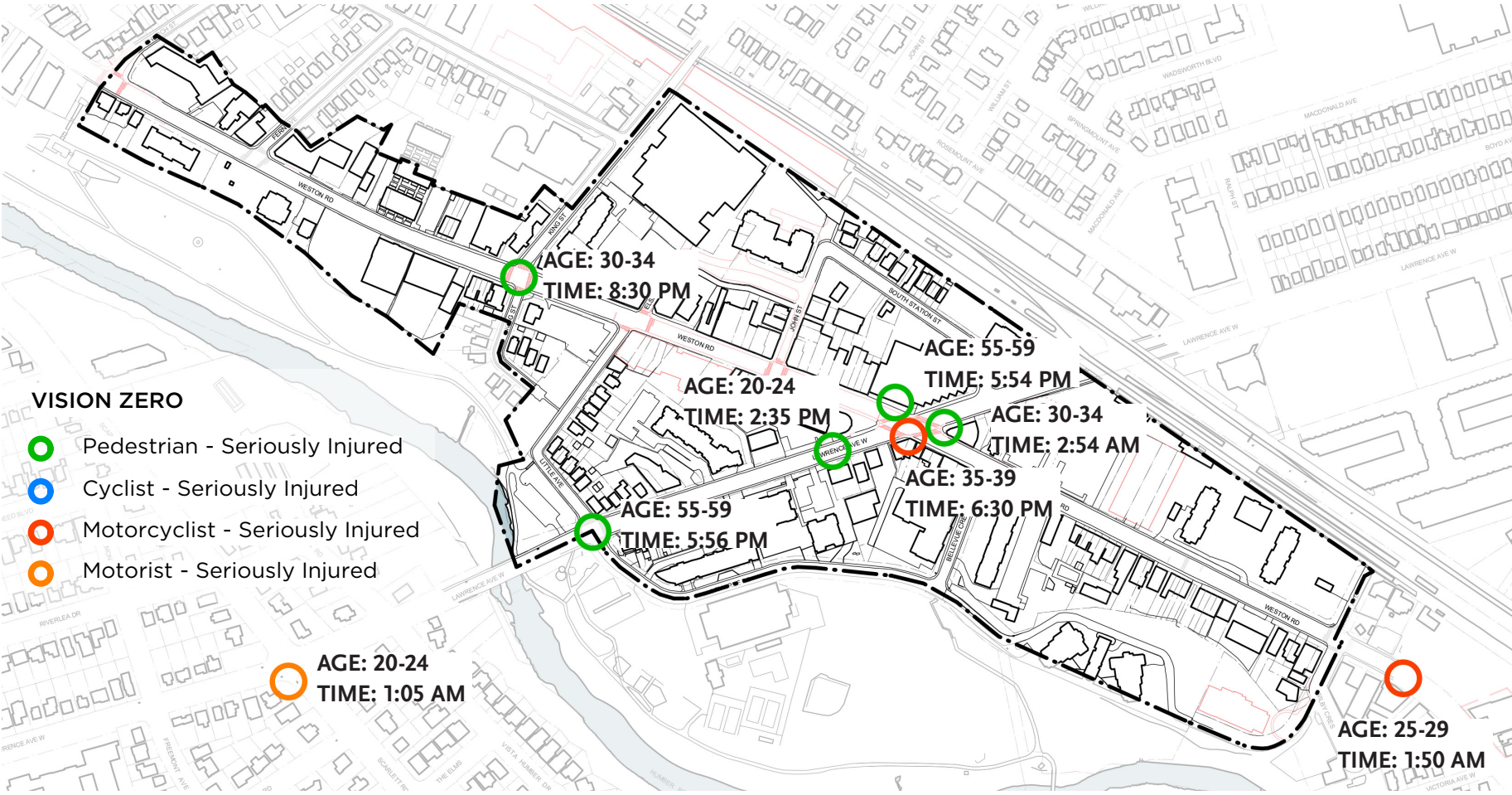
The goal of Vision Zero Toronto is to eliminate fatalities and serious injuries in our transportation system. The online mapping tool looks at KSI (killed and seriously injured) collisions to identify areas of concern and measure the City's progress.

Vision Zero's action plan focuses on six areas:

- Pedestrians
- School children
- Older adults
- Cyclists
- Motorcyclists
- Aggressive driving & distraction

The KSI mapping in the Weston area indicates that many serious injuries involve pedestrians. There are very limited collisions involving people cycling, potentially due to low cycling volumes.

**All future work in BIA must consider safety and be viewed through a Vision Zero lens.**



Source: the City of Toronto Vision Zero Mapping Tool (<https://www.toronto.ca/services-payments/streets-parking-transportation/road-safety/vision-zero/safety-measures-and-mapping/>) September 21, 2020.

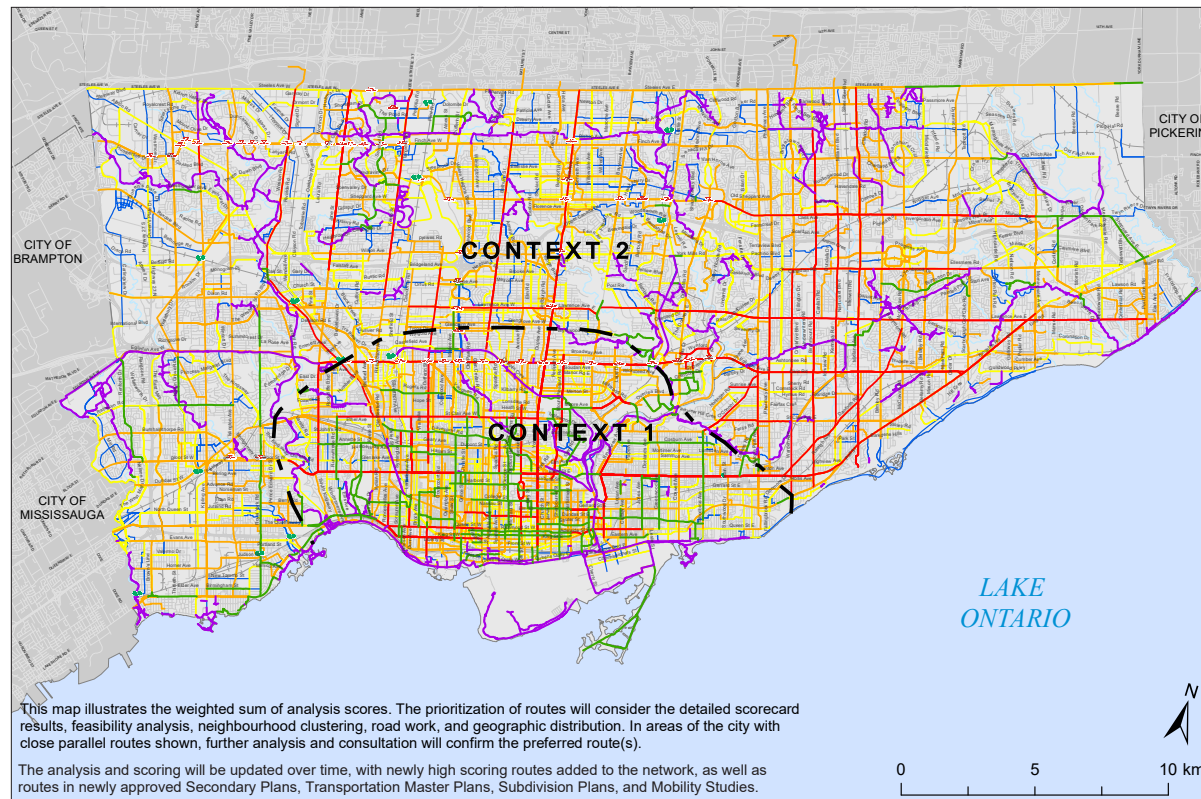
# CYCLE NETWORK ANALYSIS SCORE (2019)

The Cycling Network Analysis Score is a suitability index, the higher the score the more suitability the corridor is for future bikeways. The following streets (within the Weston Village BIA) are highlighted in the Context 2 mapping with their subsequent score from highest to lowest:

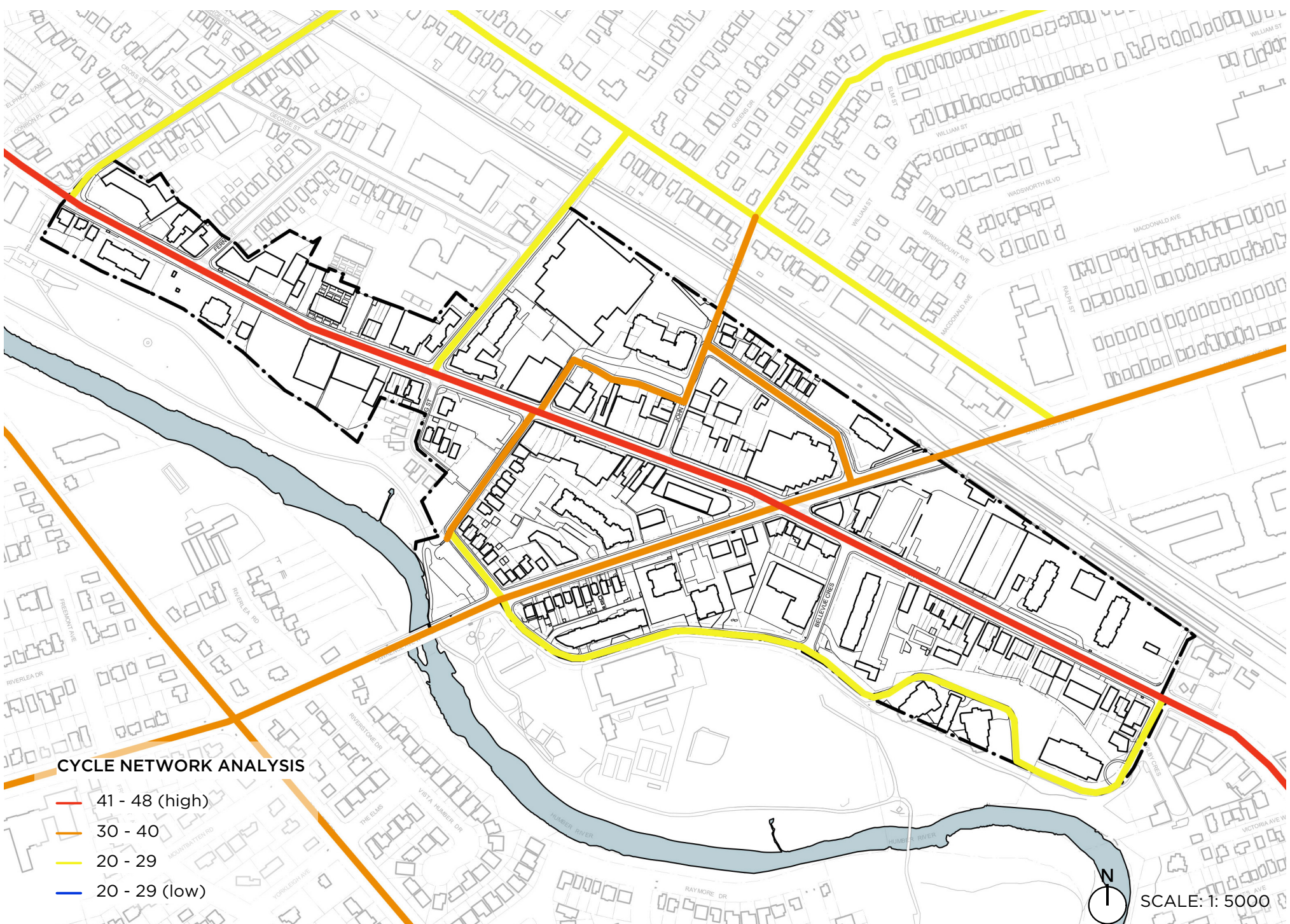
- Weston Rd. (41-48)
- Lawrence Ave. W. (30-40)
- South Station St.(30-40)
- John St. (30-40)

- Pantelis Kalamaris Lane (30-40)
- Elsmere Ave. (30-40)
- Little Ave. (30-40 and 20-29)
- Church St. (20-29)
- King St. (20-29)
- Wilby Cres. (20-29)
- Hickory Tree Rd. (20-29)

**This report considers Weston Rd. a high priority for future bikeways and infrastructure.**



*Cycle Network Analysis Score Mapping of Toronto*



Cycle Network Analysis Score Mapping overlaid on the BIA mapping

# PSCC STUDY (2018)

In March 2018 the Pedestrian Safety and Cycling Committee (PSCC) prepared a plan to improve safety and opportunities for pedestrians and cyclist in Ward 11, which includes the Weston BIA. This report includes 31 recommendations directed at the City to enhance the walking and cycling environment - 9 of these recommendations pertain to the Weston BIA, and are shown in bold below.

## FOUR FOCUS AREAS

A Safe Pedestrian is a Happy Pedestrian

- A safer walking environment
- Clearance widths along sidewalks are of adequate size, well-lit and well-maintained
- School Safety Zones

Share the Road

- Cycling opportunities on our main roads and quiet streets
- Improvements and/or enhancements to existing routes, including those along off-street trails

Connecting People to Places (in ways that make sense)

- **Provide safe walking and cycling routes to key destinations, such as the Weston Go Station and UP Express**
- Access should be made convenient for people travelling to these locations by modes of travel other than vehicles

Additional Ideas

- Public spaces, including our park pathways, should meet **accessibility standards; those that do not should be redesigned to meet these standards**
- Inform the public of actions being taken to improve safety for road users, including cyclists and pedestrians
- **Expand Bike Share Toronto into this area**

## RECOMMENDATIONS

**Recommendation 1**

- **Increase the safety of pedestrians at Weston Rd. and Lawrence Ave. W.**

**Recommendation 5**

- **That the City review sidewalk widths on major roads in Ward 11 for the purpose of identifying those which fall below accessibility standards and develop a strategy to accelerate their widening and/or replacement**

**Recommendation 9**

- **That all new cycling infrastructure in Ward 11 be physically separated from pedestrian infrastructure where feasible**

**Recommendation 10**

- **That the City investigate the feasibility of installing physical barriers, such as decorative railing, along the Lawrence Ave. W. Bridge (east of Scarlett Rd., west of Little Ave.)**

**Recommendation 23**

- **That the City review the road network around the area of Weston Station to identify opportunities for a connection to the station by way of cycling from areas north and south of the station**

**Recommendation 24**

- **That funding be allocated in the City's 2019 Budget for the implementation of Weston Neighbourhood Connections, Project ID-83 from the City's Cycling Network Plan Project List**

**Recommendation 28**

- **That the City conduct a review of parks in Ward 11 to identify those which are not fully accessible and develop a plan to retrofit existing pathways or, if necessary, create new pathways to achieve full accessibility for all parks located in the Ward**

**Recommendation 31**

- **That the City work with the Toronto Parking Authority to expand the Bike Share Toronto program into Ward 11, with potential station locations to be identified in consultation with PSCC**



# ULI TAPS (2018)

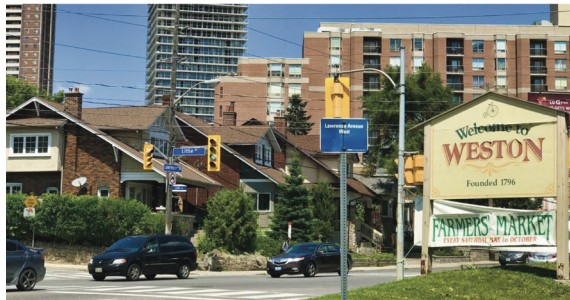
The Urban Land Institute (ULI) prepared a TAP study for the Weston BIA in 2018. This summarizes findings related to the public realm, with key elements in bold type.

## TAP VISION

- Unique Community, Distinct Identity
- Diverse and Inclusive
- **Nature-Focused**
- Fun and Vibrant
- Accessible and Connected

## WESTON TAP 2.0

Technical Assistance Panel Report 2 | September 2018



Weston Tap 2.0 cover page



## FIVE BIG IDEAS

- Promote and plan **Weston as a riverfront community**
- Market Weston as an affordable commercial and residential location for airport workers

## TAP FINDINGS

### General

- Leverage Weston Village's **built & natural heritage**
- Leverage new development
- Think beyond Weston Rd.
- Imagine Weston as a destination
- Retail and amenities
- Expand food offerings through micro-retail
- Create a culinary food hub with Frontlines
- Bring a fitness centre to Weston

### Public Realm & Infrastructure

- **Improve wayfinding and access to the river**
- Create a **central public square\***
- Creating anchors along Weston Rd.
- Beautify and animate Weston Rd.
- Consider side streets or a long-term horizon for patios on Weston Rd.
- Extend the west end bike rail path
- **Connect Weston's north and south neighbourhoods**

### Programming

- **Use cultural programming as a**

## community building exercise

- **Plan for Weston as an arts hub**

### Institutional

- Explore the idea of a community hub for social services

## ASSETS – WHAT'S WORKING WELL

- Great parks and open spaces
- Strong community sector
- Cultural and built heritage
- Improved transit service
- Private investment and increasing density
- Growing artist community
- Long-standing Farmers Market
- Community-focused local businesses

## CHALLENGES - WHAT'S NOT WORKING

- Need to reinvigorate the business area with a stronger BIA
- Socio-economic issues
- Poor pedestrian environment on Weston's main street
- A growing food desert
- Geographically and socially divided neighbourhood
- Lack of daily needs retail
- Difficult retail environment

\* **Though this was proposed, the BIA does not favour a central public square due to ongoing issues with behaviour and maintenance of existing gathering areas, and would prefer to encourage more use of the Artscape plaza.**

# JOHN STREET REVITALIZATION REPORT (2013)

The ULI facilitated a public realm improvement plan for John St. in conjunction with anticipated development. The report serves as a guide for future City public realm projects and community led initiatives. The interest expressed by participants during the meetings and design workshop was to make the public realm more inviting, attractive and celebratory. The public realm should provide opportunities to tell more of the Weston story – both the past but also the Weston of today and into the future.

## DESIGN PRINCIPLES

The streets, lanes and supporting open spaces should become critical components of the public realm network. They should better accommodate pedestrians and provide a higher quality of design and materials.

- **Encourage a more walkable Weston Village**
- **Make streets as places**
- **Build a green Weston Village**
- **Stimulate cultural expression**
- **Create identifiable landmarks**

## PUBLIC REALM PLAN – RECOMMENDATIONS

- Sidewalks and curbs: **2.1 m clear pedestrian zone on all streets.** Provide rolled or chamfered curbs.
- Roadway: narrow lane widths to gain

space for sidewalks and calm traffic. Materials within the roadway should be of a higher quality such as granite cobbles or concrete interlocking unit pavers.

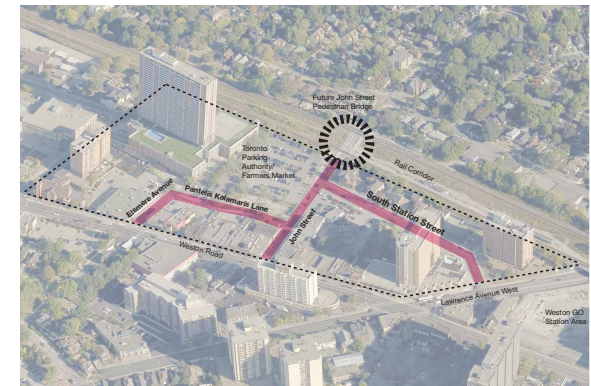
- **Street lighting:** elegant, of pedestrian scale and provide a sense of intimacy and security – especially along John St.
- **Street trees:** should be planted on all streets where the sidewalk width is minimum of 3.5 m, including John St., South Station St. and Elsmere Ave.
- **Special features:** outdoor seating, public art, public open spaces and wayfinding.

## NEXT STEPS

This public realm improvement plan is an outreach effort by ULI and partners for the community of Weston. It currently has no official status. The recommendations within were offered as a tool for the community, the local Weston Village BIA, and the City of Toronto for moving forward through the redevelopment and revitalization of Weston, and to coordinate with the many other initiatives currently underway in the community.

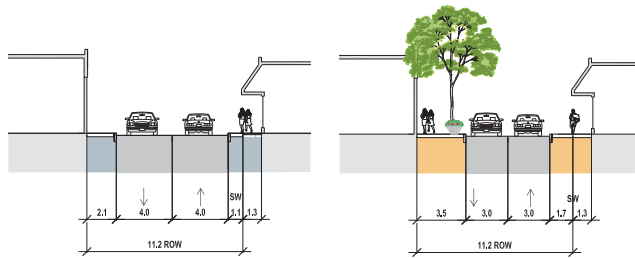


**dtah**  
*John St. Revitalization Report cover page*



*Areal mapping of Elsmere Ave., Pantelis Kalamaris Lane, John St. and South Station St.*

Street Design > Elsmere Avenue



- Existing
- 2 through lanes
  - Wider roadway than required
  - Narrow, sub-standard sidewalks
  - overhead wires
  - no trees

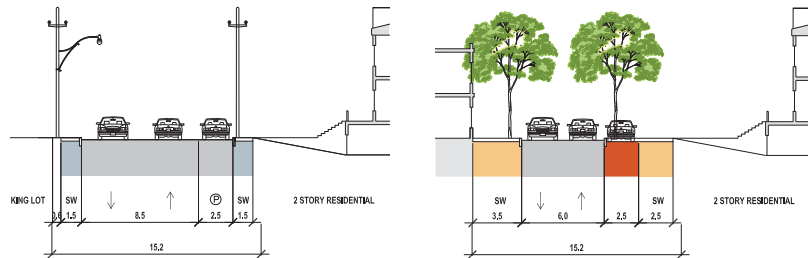
- Potential
- 2 through lanes
  - widen north sidewalk to provide minimum space for planting trees
  - widen south sidewalk to improve pedestrian environment
  - narrow roadway
  - Parking lane still possible but will limit number

Elsmere Ave. right of way section



Rendering of John St. and Pantelis Kalamaris Lane

Street Design > South Station Street



- Existing
- 2 through lanes with parking lane
  - Wider roadway than required
  - Narrow, sub-standard sidewalks
  - overhead wires
  - no trees

- Potential
- 2 through lanes with parking laybys where possible
  - widen west sidewalk with redevelopment to provide minimum space for planting trees
  - widen east sidewalk to improve pedestrian environment and plant trees where possible
  - narrow roadway
  - More space for parking and trees still possible with redevelopment and driveway consolidation

South Station St. right of way section



Areal rendering of Elsmere Ave., Pantelis Kalamaris Lane, John St. and South Station St.

# WESTON 2021 - TAPS (2012)

In February 2012 the Urban Land Institute (ULI) issued a report called Weston 2021, which presented improvements for the future of Weston based on research completed through a 2 1/2 day design charrette and ULI TAP Workshop. The intent of the initiative was to advance the investment in Weston over the next decade.

This report predates the current GO Station. The outcome of this report promotes the addition of many small interventions throughout the BIA as opposed to one single project. **A historic “Emerald Necklace” is proposed as a link between the historic section of Weston and the Humber River. South Station St. is considered an essential link between John St. and the new GO Station.**

## BACKGROUND – WESTON

- Slow decline since the 1960’s.
- Magnet for new immigrants to the City.
- Crime is an issue although the perception is worse than the reality.

## SPECIFIC AREAS STUDIED

1. Weston Town Centre/John St.: TPA parking lot.
2. New GO Station: The Metrolinx.
3. Tower Renewal: 1765 + 1775 Weston Rd.

## A. DEVELOPMENT POTENTIAL

### THE CHARETTE’S VISIONS:

- Make it easier and more convenient for people to move around and through the neighbourhood.
- Cosmetic upgrades and more desirable retail to Weston Rd.

## B. TAP STUDY AREA 1

### FINDINGS

- **Weston Farmers’ Market is of significance to the community and that it should continue.**

### RECOMMENDATIONS AND NEXT STEPS

- The TPA site and surrounding area could be developed and improved over time in phases. Three phases were identified: improving the streetscape (ph. 1), relocating Farmers’ Market (ph. 2), and develop the TPA site (ph. 3).

## C. TAP STUDY AREA 2

### RECOMMENDATIONS AND NEXT STEPS

- The station site and surrounding area could be development and improved over time in phases over the next 10-15 years: the new station (phase 1), initial modest redevelopment of the site (ph. 2), signature development (ph. 3), and high-density development (ph. 4).

## D. TAP STUDY AREA 3

### FINDINGS

- Before there could be new development on the site the property owners need to improve the existing conditions.

### RECOMMENDATIONS AND NEXT STEPS

- Phased suggestion: activate central green space and make it family friendly (ph. 1), major capital improvements to existing building and grounds (ph. 2), and set stage for new condo or new rental buildings (ph. 3).

## CONCLUSION

- Weston needs to change its image through a series of quick-start community initiatives.
- The TAP suggested the City consider quick-start initiatives like marketing Weston’s great assets; improving neighbourhood safety; **adding new attractions; improving connectivity;** encouraging lively programming of public spaces; **improving Main St.;** introducing development incentives; and making a commitment to frequent and affordable transit. These public investments can then set the stage for private investment that can also support community improvements. Suggestions for long-term catalysts were also made.



February 7, 2012

# WESTON 2021

ULI TECHNICAL ASSISTANCE PANEL REPORT

*Weston 2021 Taps Report cover page*

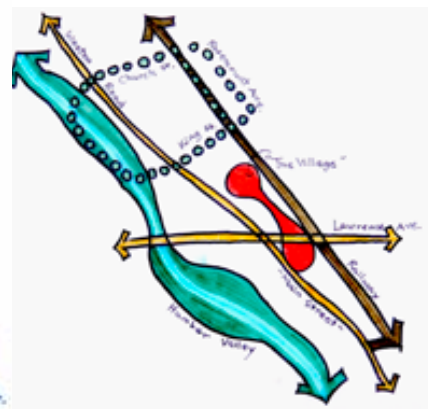
### The Charrette perspective on revitalization:



### The TAP perspective on revitalization:

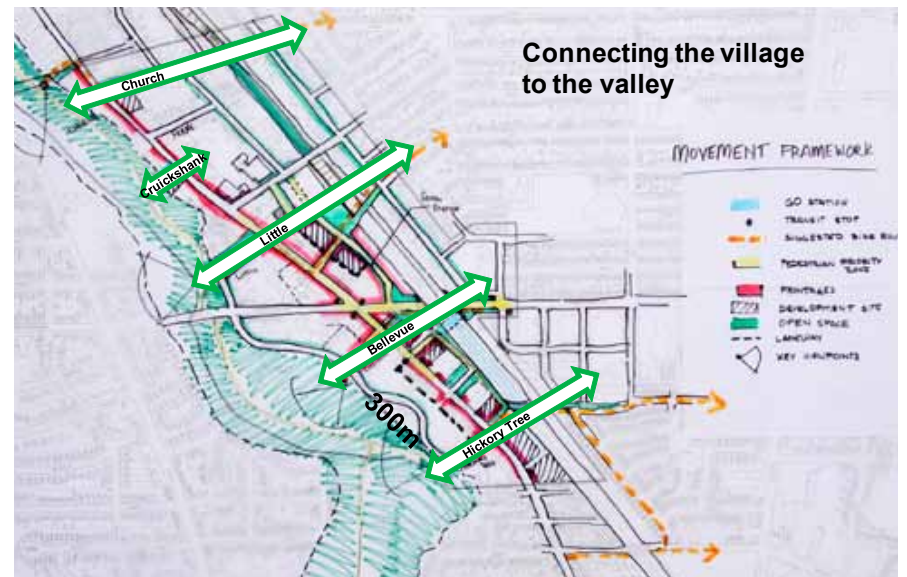


*Diagram of the different perspectives on revitalization between the charrette and TAPs*



*Sketches showing the proposed neighbourhood connectivity improvements including South Station St. between the Go Station and John St. and the "Emerald Necklace" connecting the neighbourhood to the Humber Valley.*

### Connecting the village to the valley



*Neighbourhood connectivity diagram from the Weston 2021 Design Charrette*



Photograph taken at King St. and Weston Rd.

A detailed street map of the Weston Village Business Improvement Area (BIA). The map shows a network of streets including Weston Rd, South Station St, and various residential streets like Rosemount Ave and William St. A large area in the center is outlined with a thick, dashed black line, indicating the BIA boundary. The Humber River is shown flowing through the area. The text 'EXISTING CONDITIONS/SITE ANALYSIS' is overlaid in the center of the map.

# EXISTING CONDITIONS/ SITE ANALYSIS

# HISTORICAL PROFILE

The area that is now Weston Village is part of the traditional territory of many First Nations including the Mississaugas of the Credit, the Anishnabeg, the Chippewa, the Haudenosaunee and the Wendat peoples.<sup>1</sup>

Beginning in the 1790s, the west bank of the Humber River saw the arrival of European settlers attracted by the vast forests and the industrial potential of the water, and the area's first saw mill was opened by John Conrad Countryman in 1792.<sup>2</sup> A small hamlet known as 'The Humber' grew up around this mill, which operated and expanded continuously until 1916.<sup>3</sup> In 1814, the mill was sold to a prominent local resident named James Farr, who later renamed the hamlet 'Weston', after his ancestral English home.<sup>4</sup>

In the mid-1800s, continuous flooding led to the consolidation of Weston on the table land east of the Humber River.<sup>5</sup> A wood plank road, the Weston Plank and Gravel Rd. was constructed beginning in the 1840s and the Grand Trunk Railway built the first rail lines through the village in 1856. The town flourished, with timber and grist milling, iron founding, blacksmith's shops, tanneries, breweries and distilleries, wagon works, and many other smaller industries and shops establishing themselves in the years leading up to Weston's incorporation as a village in 1881. The town

hall, known as Dufferin Hall, was built four years later and served as the civic heart of the growing town.<sup>6</sup>

The railway brought considerable industrial expansion to Weston, and by the early twentieth century it was a major manufacturing center. Iconic companies such as the Canadian Cycle & Motor Co. (CCM), the Dominion Bridge Co., The Moffat Stove Co., and the Kodak Co. established themselves in and around the area that is now the Weston Village BIA.<sup>7</sup> A sign hung over Main St. (now Weston Rd.) during this era, proudly proclaiming the town as "The Ideal Location for Home & Industry."<sup>8</sup>

In 1954, Hurricane Hazel flooded the Humber River Valley, causing many deaths and considerable damage to buildings in the lowlands and bridges across the river. This event prompted the creation of the Toronto and Region Conservation Authority and the conversion of many acres of previously occupied land into the current system of parks lining the Humber River.<sup>9</sup> In 1967, the town of Weston was incorporated into the Borough of York.

As industrial freight began to switch from rail to truck, Weston began to see the slow decline of its manufacturing industries as some factories relocated to more modern

facilities along major highways and others were supplanted by foreign competitors or fell victim to changing markets.<sup>10</sup> In 1998, York and Weston were amalgamated with the other five boroughs of Metro Toronto to form the megacity, by which time Kodak was the only large manufacturer still in operation.

Today, Weston is a neighbourhood in transition, with many future developments poised to bring a new identity to an area that was long a manufacturing powerhouse.<sup>11</sup>

## NOTES

<sup>1</sup> <https://www.toronto.ca/city-government/accessibility-human-rights/indigenous-affairs-office/land-acknowledgement/>

<sup>2</sup> <http://www.torontohistory.net/holly-farr-wadsworth-mill/>

<sup>3</sup> <https://www.toronto.com/news-story/5551605-etobicoke-history-corner-wadsworth-milling-empire-ruled-weston-area-for-87-years/>

<sup>4</sup> Weston Historical Society. 1981. *A Pictorial History of Weston*. Toronto: University of Toronto Press.

<sup>5</sup> <https://onegalstoronto.wordpress.com/2016/04/30/welcome-to-weston/>

<sup>6</sup> Robinson, Christopher Blackett. 1885, *History of Toronto and County of York, Ontario*. Toronto: C. Blackett Robinson.

<sup>7</sup> <https://library.ryerson.ca/asc/2015/01/kodak-in-toronto-1899-2005-a-century-of-traces/>

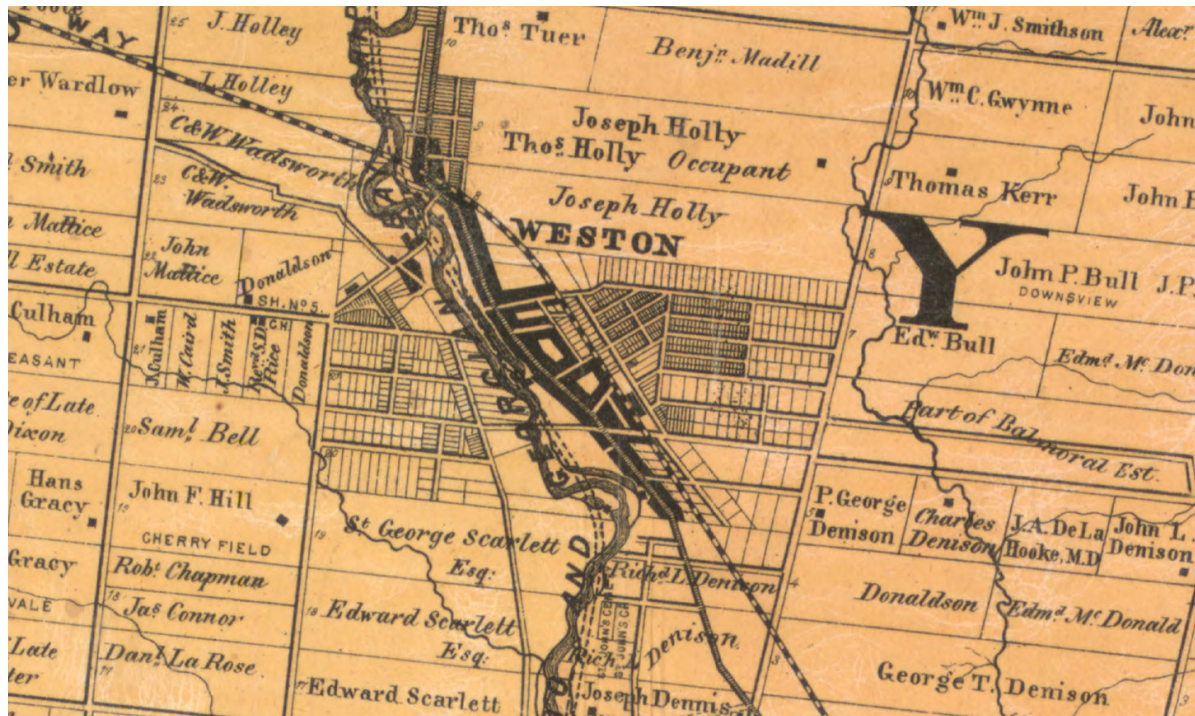
<sup>8</sup> Weston Historical Society. 1981.

<sup>9</sup> <https://www.thestar.com/news/gta/2016/10/15/hurricane-hazel-was-torontos-perfect-storm.html>

<sup>10</sup> [https://www.thestar.com/news/gta/2009/04/26/last\\_chance\\_for\\_weston\\_torontos\\_rustbelt.html](https://www.thestar.com/news/gta/2009/04/26/last_chance_for_weston_torontos_rustbelt.html)

<sup>11</sup> Gomez, Raphael; Isakov, Andre; & Semansky, Matthew. 2015. *Small Business and the City: The Transformative Potential of Small Scale Entrepreneurship*. Toronto: Rotman-UTP Publishing.





Clockwise from Top Left:

The Town of Weston, Tremain's 1860 Map of York;  
 Wadsworth's Mill, 1870; Weston Foundry & Machine Shop  
 c. 1900; Main St. (Weston Rd.) looking north, c. 1910;  
 Lawrence Ave. W. in the wake of Hurricane Hazel, 1954;  
 Aerial view of Weston Rd. and Lawrence Ave. W., late 1950s

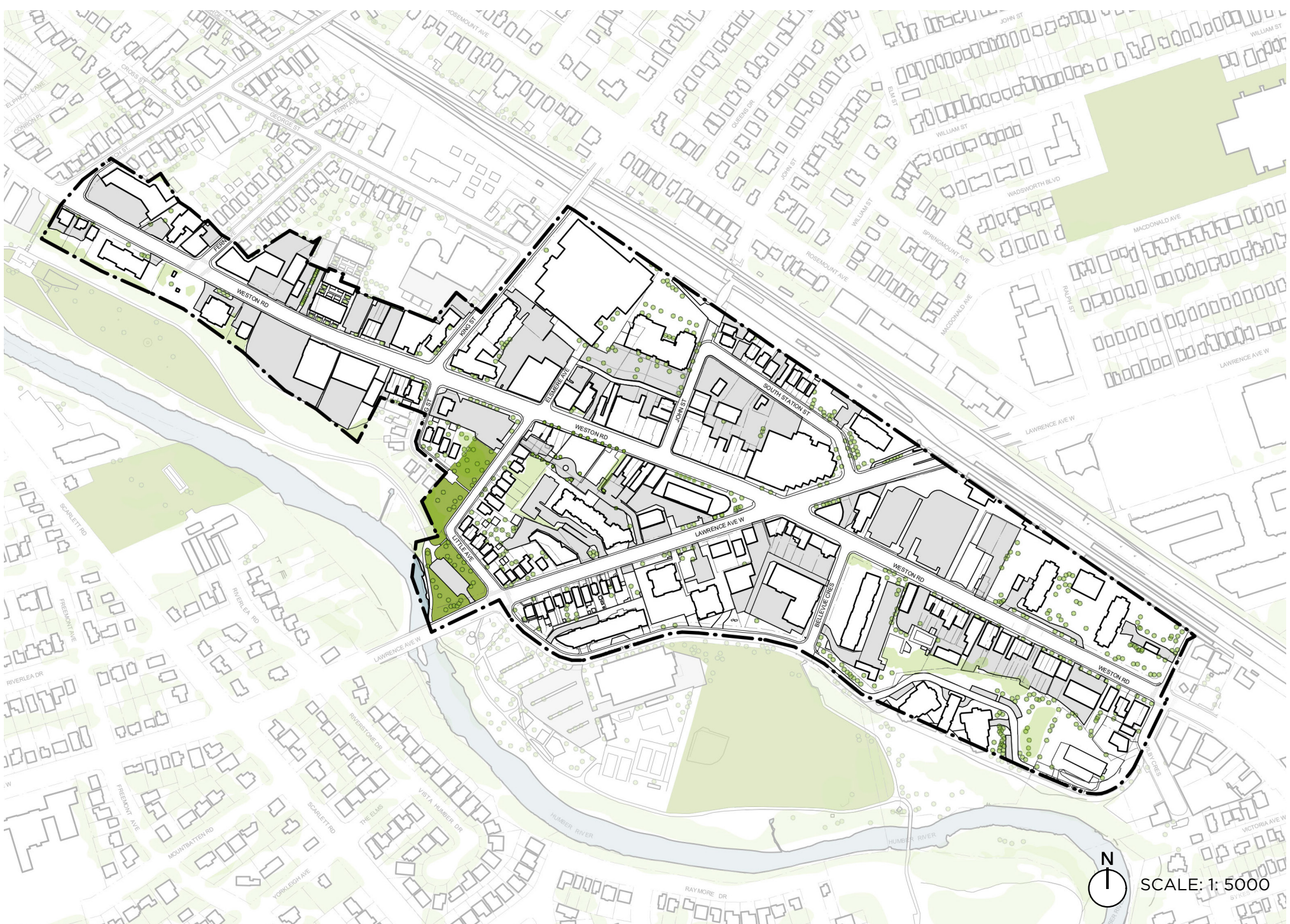


# EXISTING CONDITIONS

Weston Village, like many neighbourhoods in Toronto, was at once a self governing village remote from Toronto. The last 50 years has seen drastic change. In addition to being absorbed into the metropolitan area and its administrations, Weston has lost its strong manufacturing core (CCM, Kodak amongst others), and its cohesion, as a significant amount of its main street scale and continuity has been disrupted with strip malls, high rise residential, and parking lots over the last 40 years.

Weston Rd. has become car dominated and is not very pedestrian or bike friendly. A substantial number of mid and high rise apartments (many rental and assisted) has changed the demographics from its early days. Today the demographics are changing again as younger families seeking more tolerable housing and condo prices are moving north.

There are great historical pockets, and new regional infrastructure. This place has undergone great change and is looking for ways to re-gel and find coherence. On the following pages, we review the existing conditions in detail.



SCALE: 1: 5000

Mapping of the Weston Village BIA showing the current conditions.

# EXISTING LAND USE

- Existing land use in Weston is divided into disconnected retail pockets due to the way the land was sold and redeveloped. Older two story buildings, often with second and third generation businesses, occupy about 1/3 of Weston Rd. in distinct strips – the west side of street at the south end and from Bellevue Cres. to Elsmere Ave. in two parts.
- New commercial buildings are large low-rise (like Ford), or strip malls.
- Very wide and deep parking lots hinder continuity and give the street a suburban car dominant feel, which includes the GO Station and the funeral home visitor parking lots.
- Lawrence Ave. W., Little Ave. and South Station St. have a high concentration of small single family residential homes.
- At the south end of the BIA the residential development and GO Station parking lot have expansive open areas, in contrast with the tightly knit retail use on the opposite side of the street.
- The north end of the BIA has a mix of land use with larger street setbacks.
- There is no longer any industry and very few offices.
- The residential mid/high-rises along Weston Rd. have no public retail or

services at their base, reinforcing the experience of discontinuity.

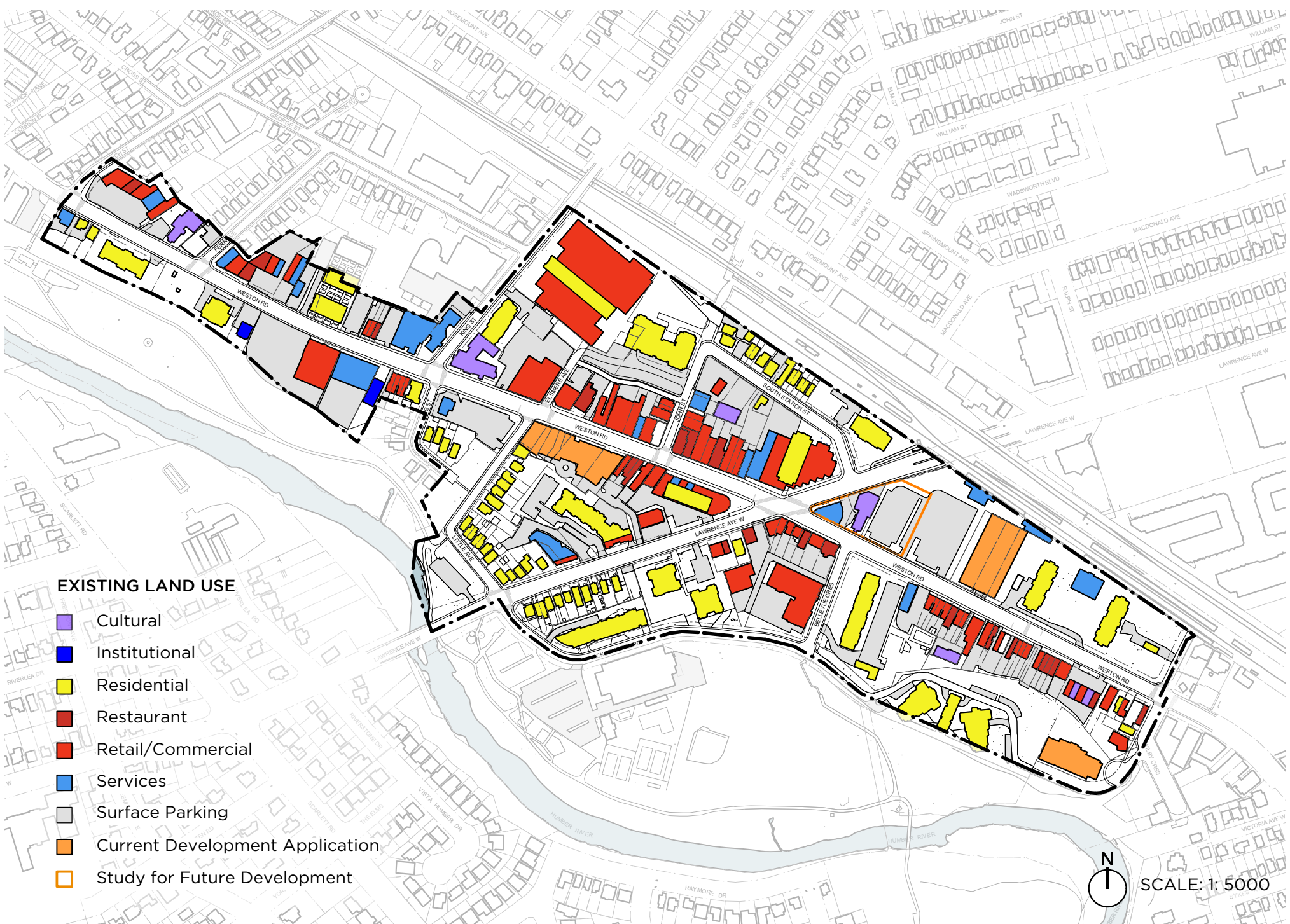
- There are many churches along Weston Rd.



*Large scale residential development*



*Low-rise retail/commercial space*



**EXISTING LAND USE**

- Cultural
- Institutional
- Residential
- Restaurant
- Retail/Commercial
- Services
- Surface Parking
- Current Development Application
- Study for Future Development

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SCALE: 1: 5000

*Mapping of the Weston Village BIA outlining existing land use*

# BUILDING HEIGHTS

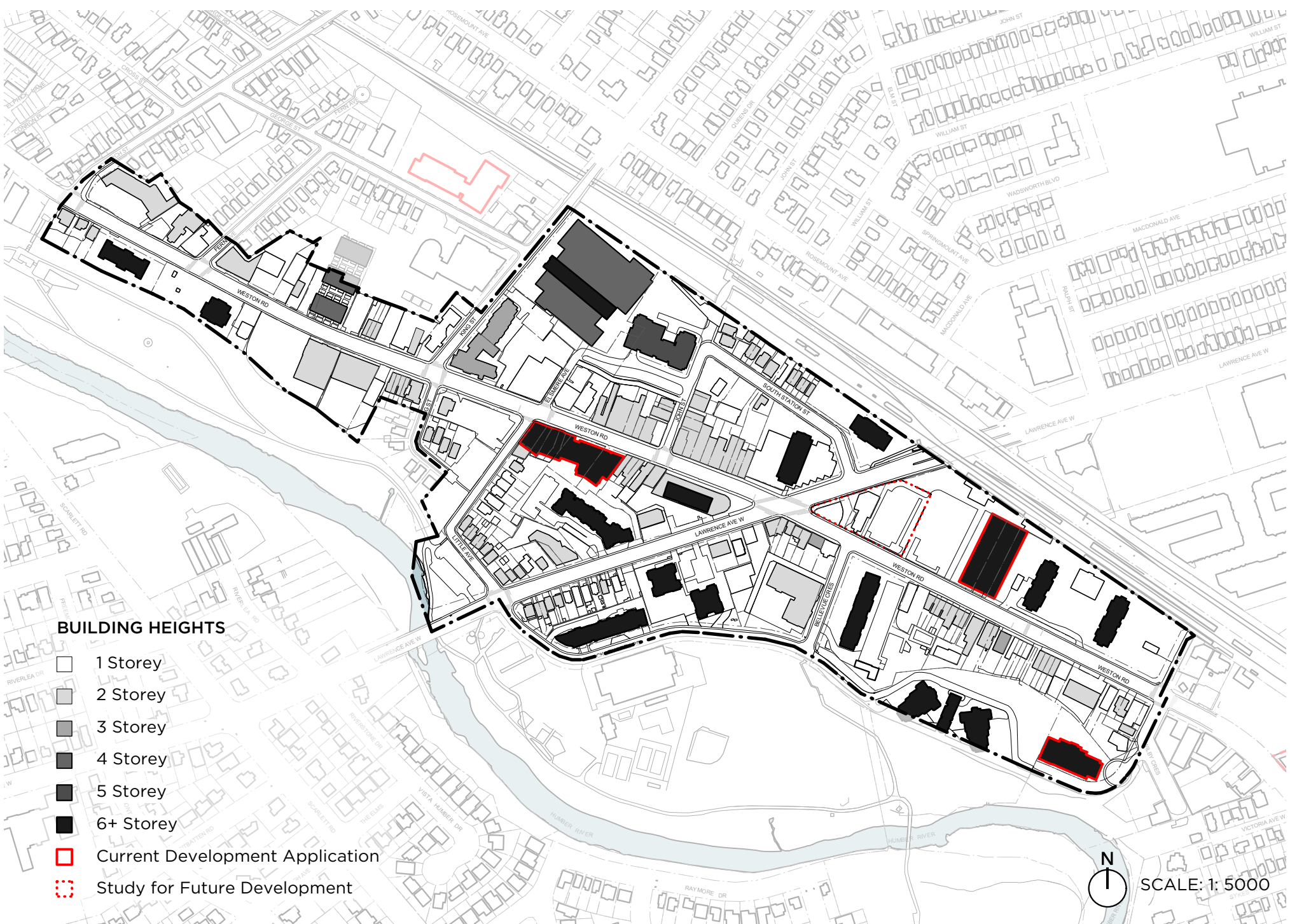
- Weston Rd. is made up of buildings with starkly contrasting and discontinuous scales with predominantly two-story retail/commercial buildings, and high-rise residential apartments.
- A number of 2-storey buildings south of Little Ave. and elsewhere are being assembled for new taller developments, primarily with podium tower typologies.



*High-rise building photographs*



*Low/Mid-rise building photographs*



**BUILDING HEIGHTS**

- 1 Storey
- 2 Storey
- 3 Storey
- 4 Storey
- 5 Storey
- 6+ Storey
- Current Development Application
- Study for Future Development

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SCALE: 1: 5000

*Mapping of the Weston Village BIA outlining existing building heights*

# CURRENT DEVELOPMENT APPLICATIONS AND SITE STUDIES FOR FUTURE DEVELOPMENT

- There are currently 3 developments in the application process at the City. They all have a “main street” podium style base made up of retail/commercial, with residential towers above.
- 1885 & 1871 Weston Rd. development is in its early stages of design and Castlepoint Numa has not yet determined a specific use for the site. Looking to a future loss of street parking due to bikeways, the BIA/City could use this opportunity to explore new Green P Parking in the development. As it is at the centre of the BIA, this is an ideal location.
- Unlike residential towers designed and built in the 1960's and 70's – new residential towers slated to be built on Weston Rd. will have a podium, therefore there will be a renewed street presence brought by new development.





VIEW ALONG GEORGE STREET

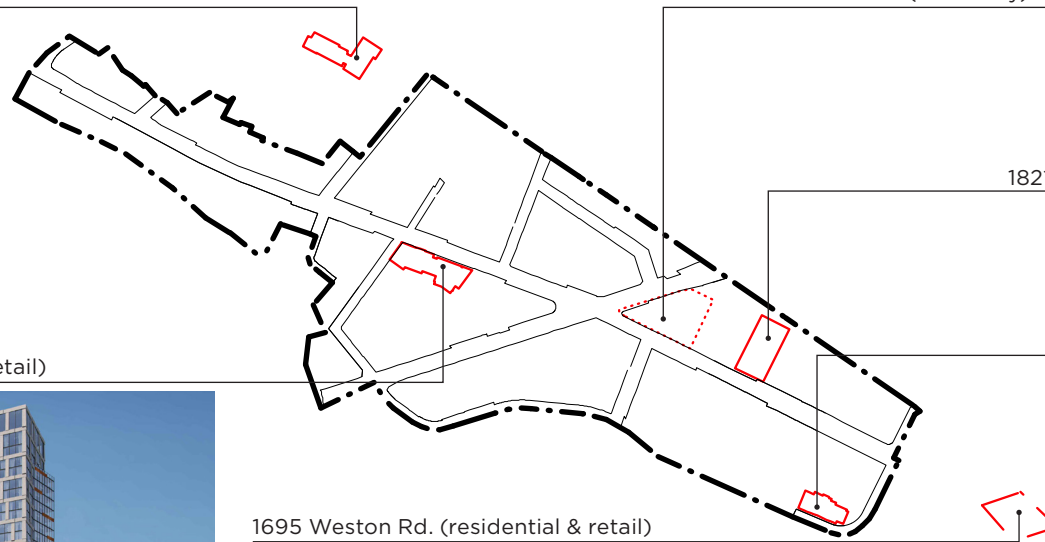
23 George St. (school)



1885 & 1871 Weston Rd. (site study)



1821 - 1823 Weston Rd. (residential & retail)



1956 Weston Rd. (residential & retail)



1695 Weston Rd. (residential & retail)



10 Wilby Cres. (residential)



SCALE: 1: 10000

# LANDSCAPE, GREEN SPACE AND PUBLIC REALM

- Weston Village has significant private green spaces surrounding large developments and large public park green spaces nearby, but there are few trees and other landscaping right along Weston Rd.
- The BIA's three parks (Cruickshank Park, Little Avenue Memorial Park and Weston Lions Park) are assets to the neighbourhood. Cruickshank Park and Weston Lions Park are currently disconnected from Weston Rd. due to the change in elevation between Weston Rd. and the Humber River.
- Memorial Park is tucked away behind a parking lot and is difficult to find unless you know it is there.
- The BIA would benefit from additional tree canopy toward the centre of the BIA, which is hard to achieve with the narrow sidewalk widths, especially north of King St. Widening the sidewalks north of King St. could allow for new tree planting. This is currently not in the City's plan.
- Some of the existing trees are in raised planter boxes, which are poor for tree health, and tend to get used for litter, however, these trees are currently in fair condition, and the raised planters are to remain.
- There are concrete planters (without

trees) distributed throughout the BIA which are broken or haggard and should be removed.

- All opportunities for new greening should be identified including partnerships for improving residential green space.
- There is significant pedestrian access from the Humber River multi-use trail, however it is poorly indicated.
- The Masonic Temple lawn provides a possible opportunity for landscaping or activity space partnership with the BIA.
- There is an abundance of seating at the centre of the BIA where the use tends to encourage all day loitering, discouraging casual use by others. The BIA would like benches to be replaced throughout the BIA. The BIA would like to spread out the seating opportunities into smaller clusters.



Trees in boxes



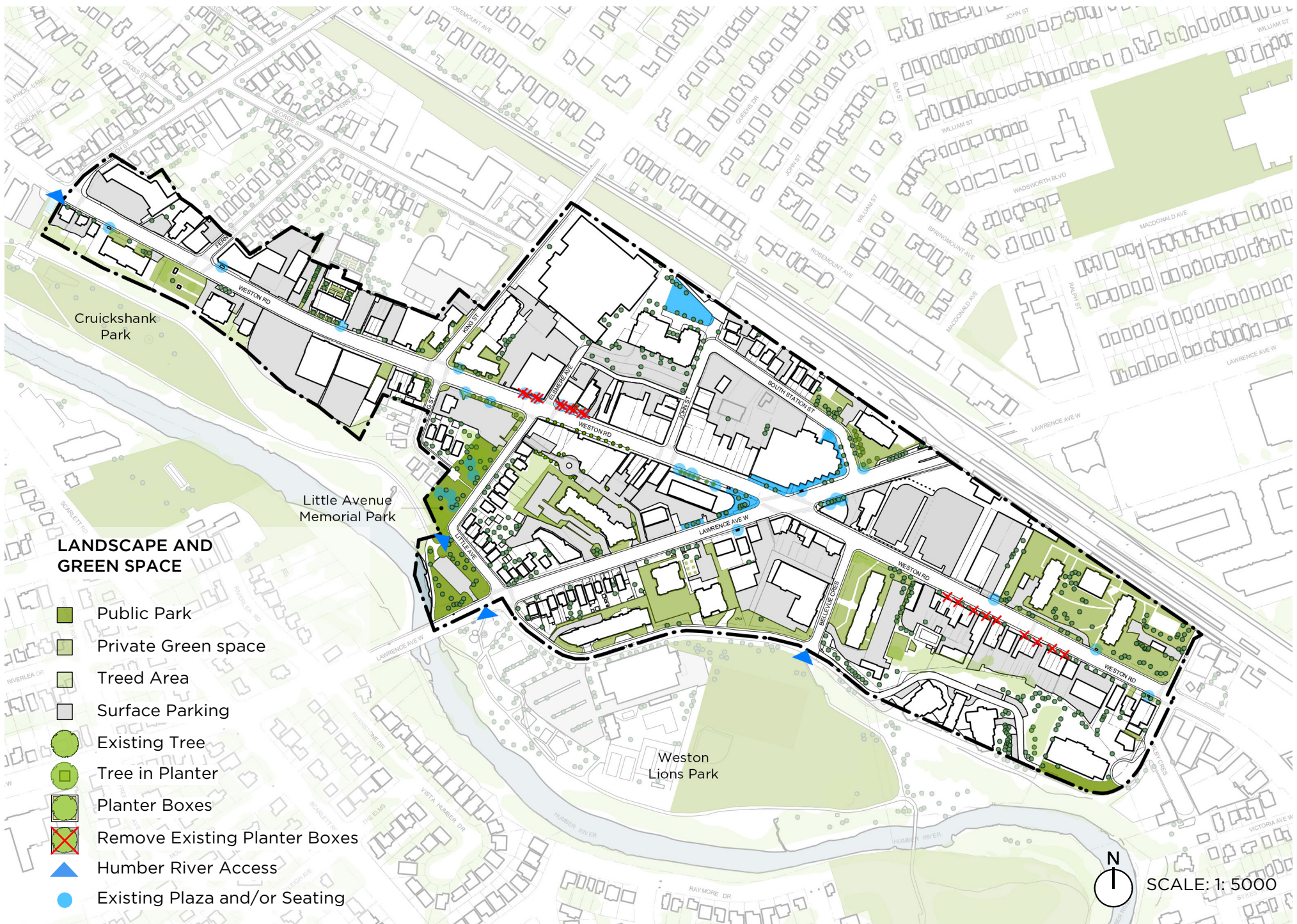
Cruickshank Park



Little Avenue Memorial Park



Weston Lions Park



**LANDSCAPE AND GREEN SPACE**

- Public Park
- Private Green space
- Treed Area
- Surface Parking
- Existing Tree
- Tree in Planter
- Planter Boxes
- Remove Existing Planter Boxes
- Humber River Access
- Existing Plaza and/or Seating

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SCALE: 1: 5000

*With future redevelopment this mapping may be subject to change.*

Mapping of the Weston Village BIA showing existing green spaces, plazas and seating areas

# TRANSPORTATION

- The BIA is well-served by transit, vehicles and cars.
- The north-south axis of Weston Rd. is largely car dominated. The east-west axis is more pedestrian – coming from the Humber, or over the rail tracks. The east-west pedestrian access and the north-south vehicular access are operating on two different axes, and are largely two different systems. The southern gateway project (proposed by the BIA) is addressing the car access but the pedestrian and bike access could use more attention.
- There is one Bike Share station within the BIA near Weston Lions Park. The only Bike Share in close proximity is south at Eglinton Ave. and Scarlett Rd. – 3 km away.
- The centre of the BIA has many bike racks, however the Humber River access locations and the north and south ends of the BIA would benefit from additional bike parking.
- Bike access up from the Humber River is inconsistent – it is important to provide signage to indicate when there is both pedestrian and bike access verses only pedestrian access.



*Car dominated Weston Rd.*



*Pedestrian access up to the table land at Bellevue Cres.*



*Pedestrian and bike friendly overpass to the GO Station*



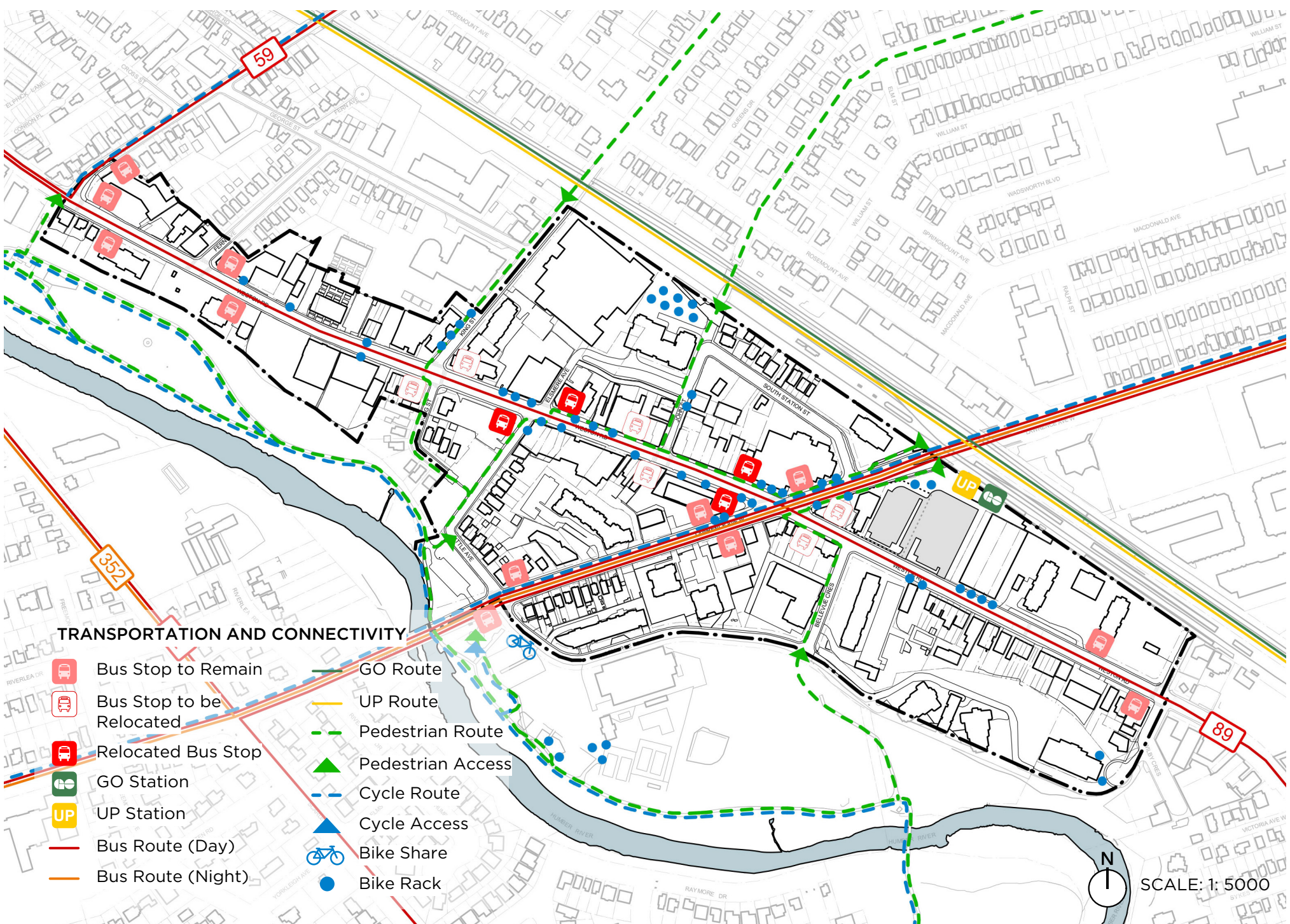
*Bike Share at Weston Lions Arena*


















*Bus stop at Weston Rd. and King St. to be relocated*



*The Humber River Multi-Use Trail*



**TRANSPORTATION AND CONNECTIVITY**

-  Bus Stop to Remain
-  Bus Stop to be Relocated
-  Relocated Bus Stop
-  GO Station
-  UP Station
-  Bus Route (Day)
-  Bus Route (Night)
-  GO Route
-  UP Route
-  Pedestrian Route
-  Pedestrian Access
-  Cycle Route
-  Cycle Access
-  Bike Share
-  Bike Rack

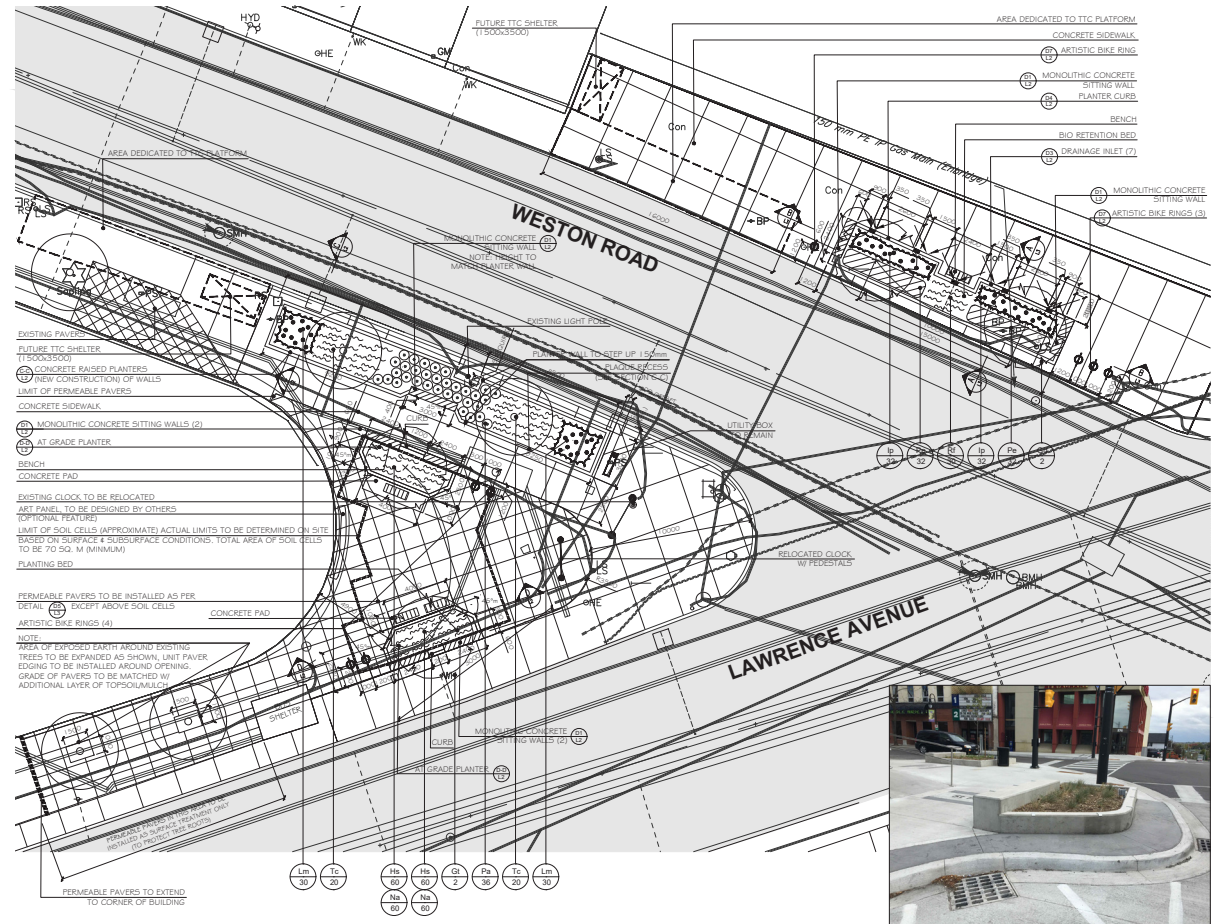
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SCALE: 1: 5000

Mapping of the Weston Village BIA showing current transportation routes

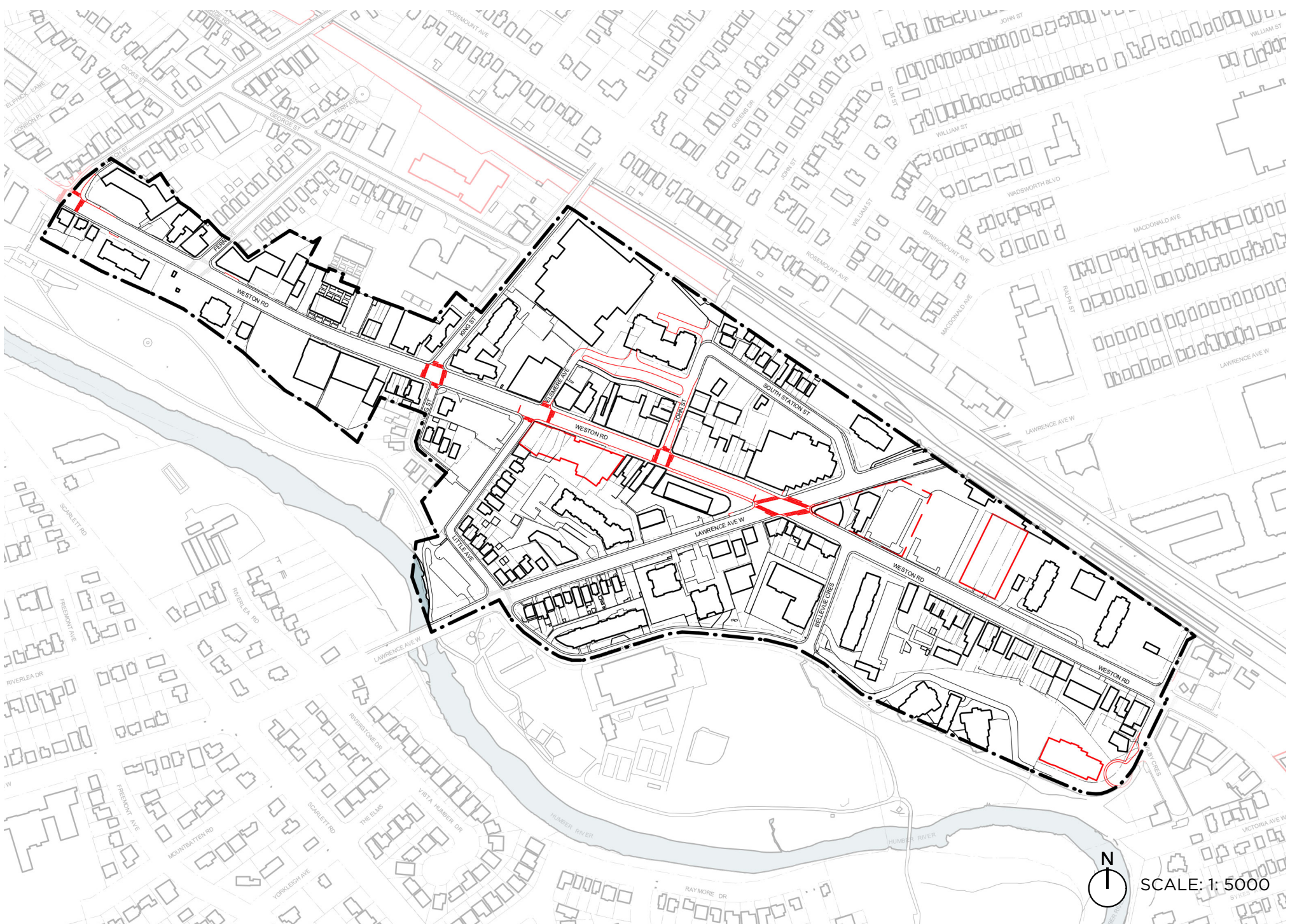
# CURRENT/RECENT CITY WORK

The City is currently making improvements in the streetscape between Lawrence Ave. W. and Church St. including:

- The City of Toronto sidewalk widening up to Elsmere Ave. and crosswalk implementation along Weston Rd. and John St.
- The Weston Rd. improvements do not currently include bikeways, however it is anticipated that they will be included in future streetscape improvements led by the City.
- New plaza and planting plans are being implemented at Weston Rd. and Lawrence Ave. W. at the northeast and northwest corners by the City of Toronto.
- The roadwork along Weston Rd. is removing parking in this area – the BIA/City should explore the potential for new municipal parking below grade within proposed new developments.
- John St. improvements are being implemented per the Public Realm Improvement Plan (2013) study.
- A rail deck has been implemented north of Church St., between Church St. and King St. and a rail trench has been constructed between King St. and John St.



City development at Lawrence Ave. W. and Weston Rd.



SCALE: 1: 5000

Mapping of the Weston Village BIA showing current/recent city work

# EXISTING STREET SECTIONS

Although the current road work along Weston Rd. does not include bikeways, they are anticipated. The City is planning bikeways for Lawrence Ave in 2021 and potentially on Weston Rd. in the near future. The desire for bikeways was a significant take away from the discussions at the public consultation meeting.

City staff asked that this report identify the right of way (ROW) widths along Weston Rd. to facilitate their future study of how to implement the bikeways. The ROW widths along Weston Rd. vary from 17 m to over 21 m. The following map outlines where these various widths occur based on the PSCC mapping. The final arrangement of elements will be up to the City when the bikeway is considered.

- Sidewalk widths within the BIA vary

widely. A 3.5m metre sidewalk width was recommended for secondary streets within the John Street Revitalization research area, however, many of the existing streets do not have this sidewalk width nor the capacity. A minimum of 2100 should be provided.

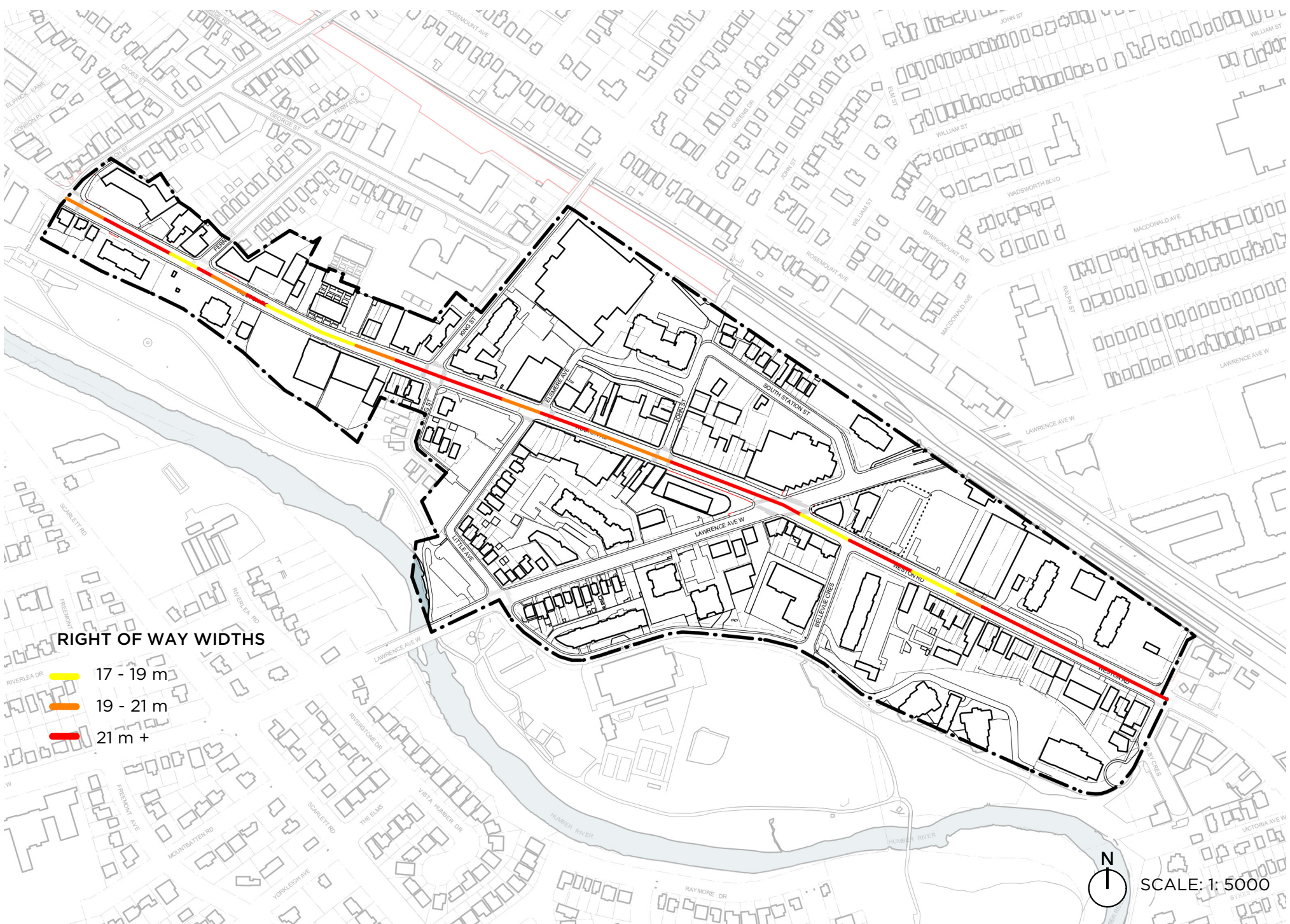
- Alignment of hydro poles and other utility poles is inconsistent along the boulevard. Any reconsideration of the sidewalks should provide alignment and a comprehensive Utility plan and meet AODA compliance for all obstructions.

Refer to Appendix B for possible section configurations based on existing R.O.W widths.



*Example of bikeway and parking bump outs, proposed for Eglinton Ave. (Midtown Yonge Study)*





Mapping of the Weston Village BIA showing existing right of way widths along Weston Rd.

# ART AND HERITAGE

- The heritage designated district within the BIA is concentrated around King St., Little Ave. and Little Avenue Memorial Park and is characterized by riverstone walls along the streets.
- The Carnegie Library and the Central United Church are strong public anchors at Weston Rd. and King St.
- Just northeast of the BIA there is the large heritage district and study area.
- Art is expressed primarily through murals throughout the BIA. Sculptures are few and concentrated around Little Avenue Memorial Park.
- The BIA's branding is based on the penny farthing. It is on the clock at the centre of the BIA, and on some pedestrian light poles and banners. They used to be on all poles, but Hydro asked to remove many.



*Penny Farthing Clock*



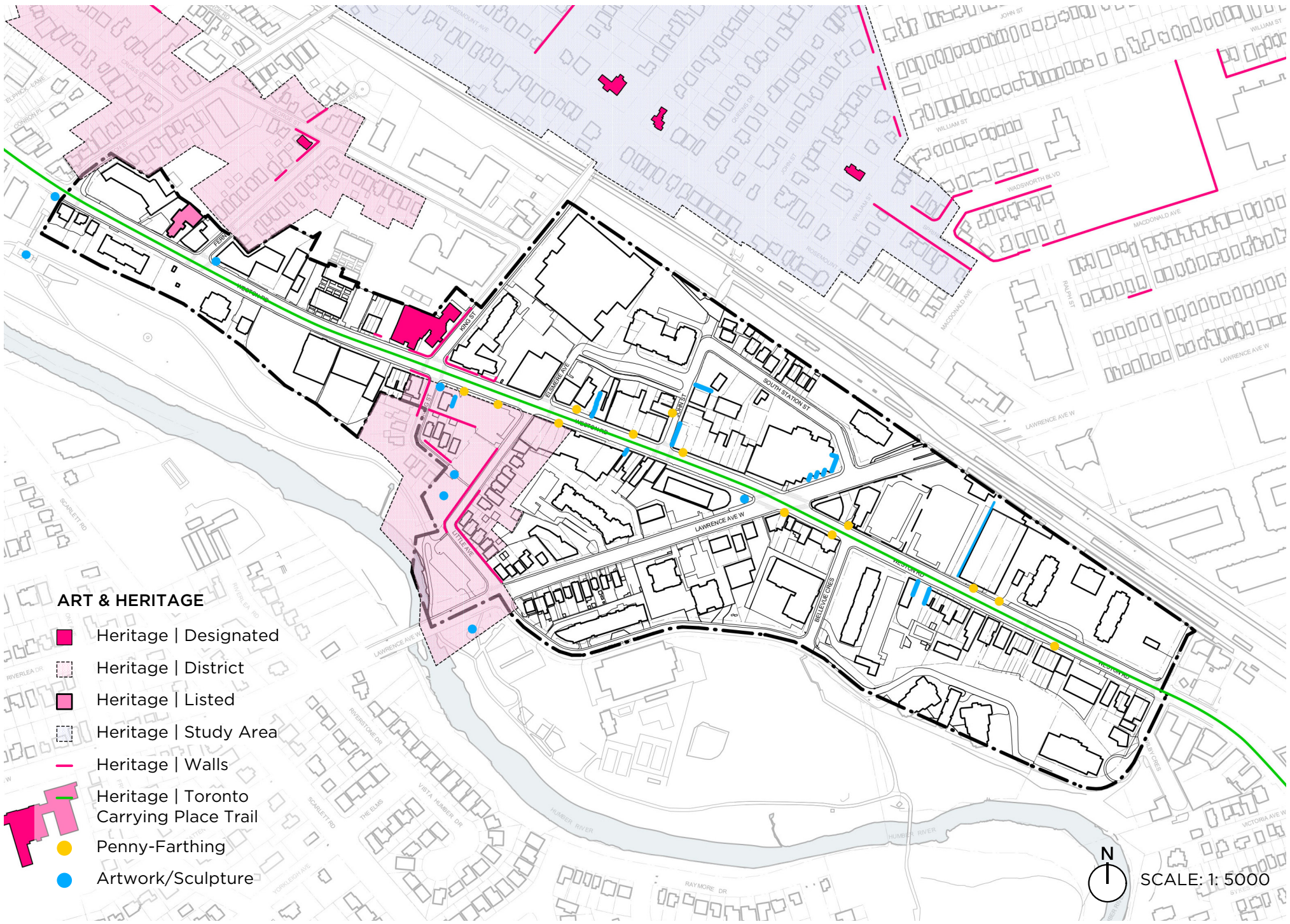
*Little Ave.*



*Murals along Weston Rd.*



*Carnegie Library*



**ART & HERITAGE**

- Heritage | Designated
- Heritage | District
- Heritage | Listed
- Heritage | Study Area
- Heritage | Walls
- Heritage | Toronto Carrying Place Trail
- Penny-Farthing
- Artwork/Sculpture

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SCALE: 1: 5000

Mapping of the Weston Village BIA showing existing heritage buildings, heritage areas and artworks

# EVENTS AND FESTIVALS

- The Farmer's Market was previously held in the GO Station parking lot with lots of visibility from Weston Rd. The Farmers Market is now located on John St., however not the Artscape Plaza as originally planned, as the farmers will not set up if they are not adjacent to their trucks. John St. provides limited event use due to the new width and fire route requirements. If any other events are desired, then a new space will be needed. The Artscape plaza provides a possible location, but has not been fully engaged by the BIA. It's lack of centrality makes it difficult to find for those who are not familiar with the area. Wayfinding from Weston Rd. is desired.
- The Santa Claus Parade runs along Weston Rd. During the parade the public occupies the northeast corner of Church St. and Weston Rd., the Central Ontario Building Trades parking lot, 2000 Weston Rd. parking lot, the Weston Park Baptist Church parking lot, and the northwest corner of Hickory Tree Rd. and Bellevue Cres. due to partnerships and available space. These sites should be considered for future partnerships with the BIA for events. As the 2000 Weston parking lot will be impacted

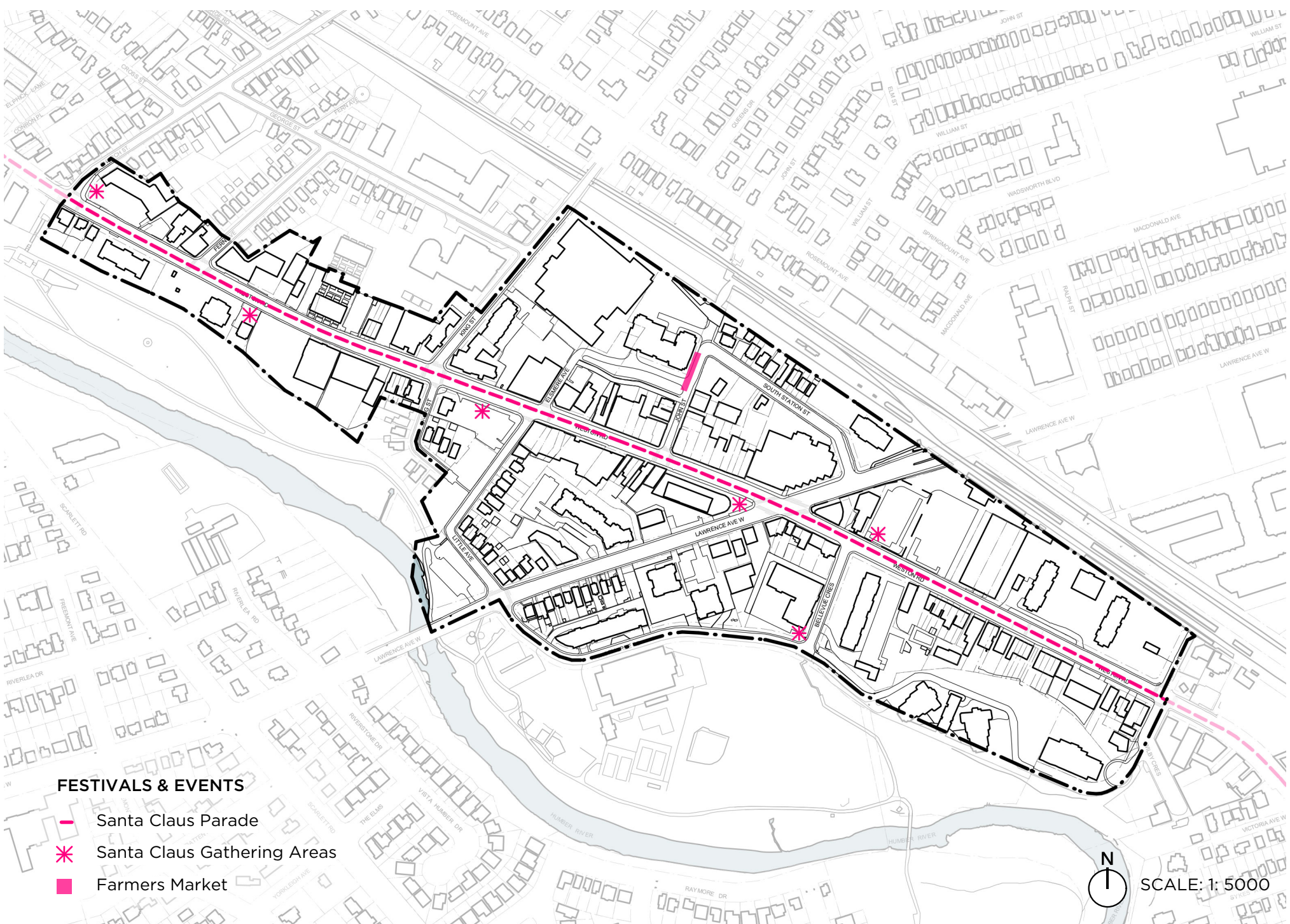
by development construction, consideration should be made for new temporary locations.

- The Weston Taps 2.0 identified the need for a new central plaza for events, however, the BIA instead favours dispersed smaller gathering areas and working with their partners to activate open space that already exists, rather than a permanent "square".



Santa Claus Parade

Farmers Market



**FESTIVALS & EVENTS**

- Santa Claus Parade
- ✱ Santa Claus Gathering Areas
- Farmers Market



SCALE: 1: 5000

Mapping of the Weston Village BIA showing existing festival and event areas

# CONNECTIVITY

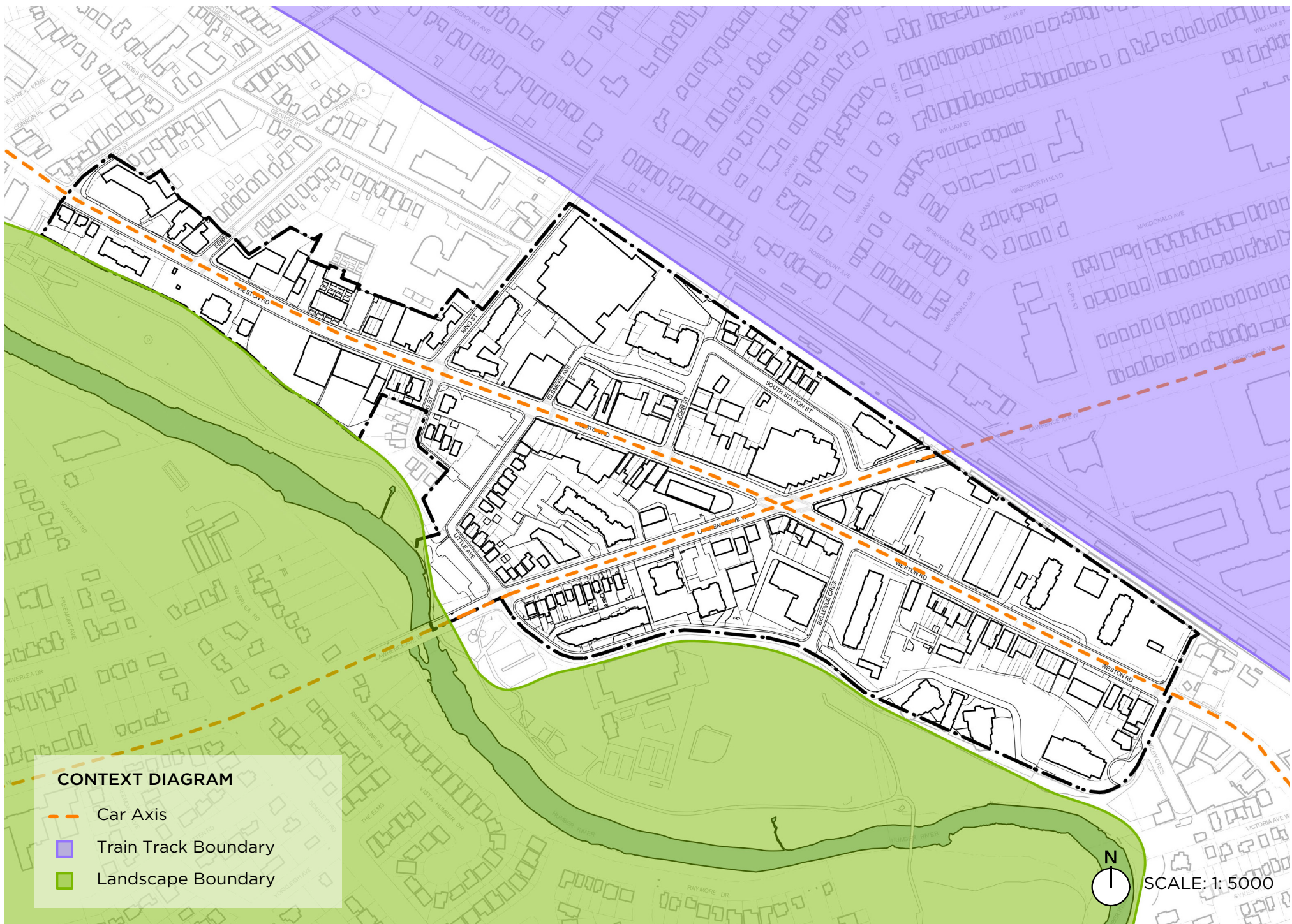
The two prevailing physical barriers in the BIA are the southwest elevation change toward the Humber River and the northeast rail (parallel to Weston Rd.). Weston Rd. runs like a spine on this plateau while significant infrastructure (tunnel and bridge) allow Lawrence Ave. W. to connect at the east and west. These are both car dominated roads. All other connections east and west require stairs, ramps and rail crossings. It is important to try to encourage connectivity across these barriers.



Humber River Barrier



Rail Barrier



Mapping of the Weston Village BIA diagramming the surrounding context

# EXISTING GATEWAYS

Currently there are four “official” gateways into Weston Village – each from a different era with a different aesthetic, lacking a coherent vision.

- The existing BIA’s gateways have signage/beacon qualities that help to denote entry into the district. Some of these gateways need to be refurbished.
- The gateway concept at Wilby was only partially constructed and does not currently appear as an “entry”.
- There are many opportunities here for providing new gateways that address pedestrian and cyclists to improve connectivity. These locations have been identified in the map.
- Gateways should have a more common language that can provide consistent wayfinding that signal entering and exiting the BIA.





**GATEWAYS (EXISTING AND POTENTIAL)**

- Potential Gateway
- Signage/Beacon
- "Town Square"
- ▲ Pedestrian Access
- ▲ Cyclist Access
- ▲ Car Access

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SCALE: 1: 5000

Mapping of the Weston Village BIA showing existing gateways



# THE MASTER PLAN

Rendering of the south gateway proposed at Church St. and Weston Rd.

# MASTER PLAN GOALS AND PRINCIPLES

The master plan recognizes the patchwork aspects of the area, and its relative isolation due to topography and rail tracks. The main goals are to:

- Increase the vitality of the streetscape and help create a cohesive pedestrian experience that enhances commercial activity by improving lighting, pedestrian connection, park linkages, retail improvement, laneway revitalization, and
- Create cohesion along the length of the street through consistent approaches to planting, furniture, and artwork elements which can be repeated throughout the BIA to bridge existing gaps in the commercial fabric.

## PRINCIPLES

The guiding principles aim to strengthen connectivity between the patches and east-west access through the BIA:

### 1. ENHANCE CONNECTIVITY

- Encourage continuity and enhance public space along Weston Rd.
- Encourage east-west access across the BIA cross streets and laneways
- Provide wayfinding and gateways for pedestrians, cyclists and cars
- Activate side streets
- Promote bike usage
- Strengthen the existing gateways through lighting, new artworks and landscape enhancements

### 2. HIGHLIGHT HUMBER RIVER ADJACENCY

- Promote access and wayfinding to and from the river and multi use trail
- Highlight history and ecology
- Promote a strong green agenda

### 3. AGGREGATE OF SMALL GATHERING NODES

- Distribute seating across the BIA in small clusters

### 4. VEGETATION OUT OF HARM'S WAY

- Maximize new tree planting/tree health
- Promote seasonal planting strategies that do not attract litter

### 5. HISTORIC NARRATIVES

- Showcase historical narratives and artifacts
- Educate and capitalize on the historical themes of the neighbourhood: CCM industry, riverstone walls, history of indigenous peoples and the Toronto Carrying Place Trail

### 6. IT CAME FROM THE LAND AND WESTON MADE IT INTO SOMETHING

- Honour the manufacturing past by highlighting materials and fabrication
- Celebrate the culture of bikes: promote bike culture through festivals and artwork

# MASTER PLAN

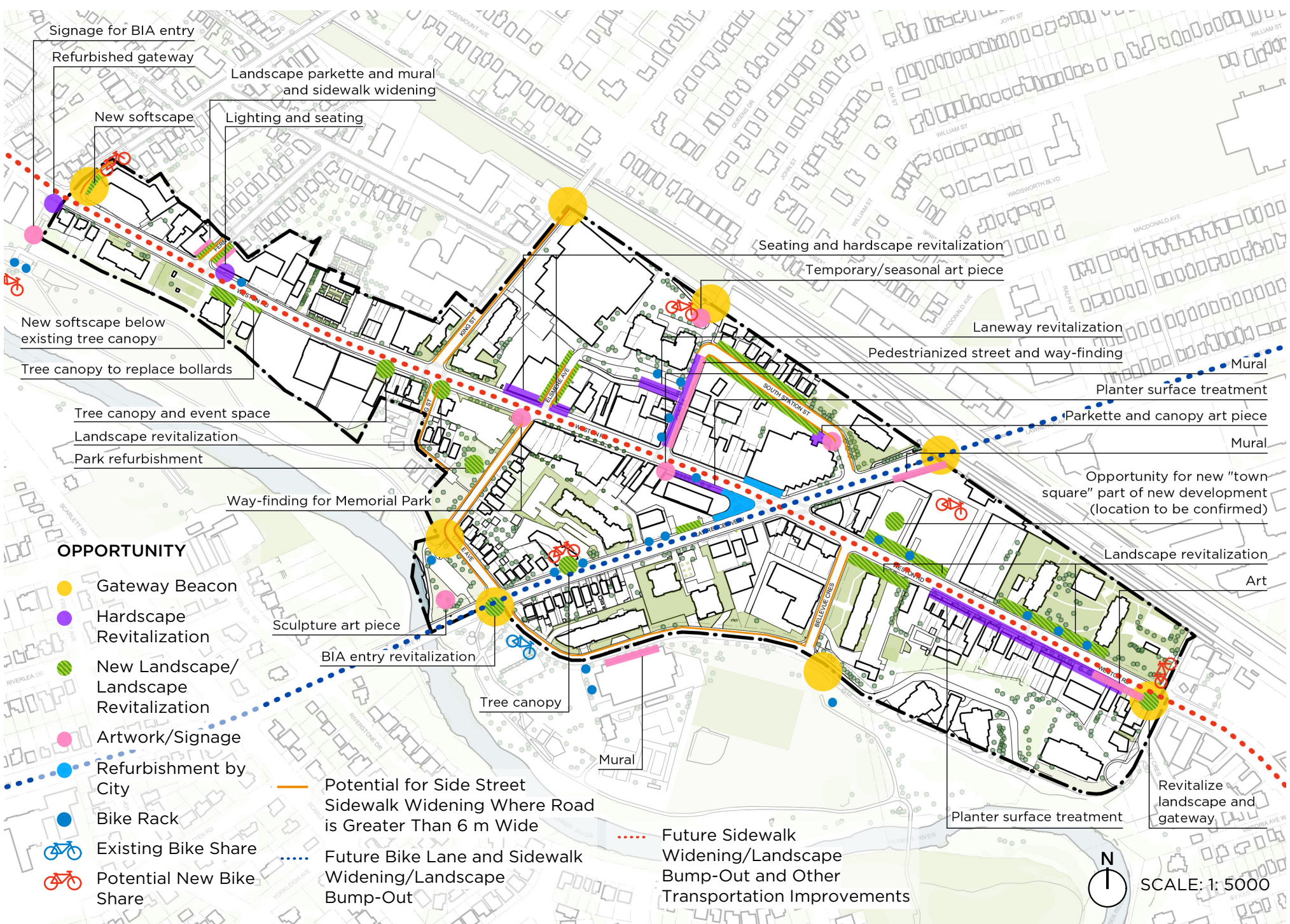
Interventions and improvements proposed in this document aim to enhance the vitality of the streetscape along Weston Rd.

- Opportunity sites are shown as a patchwork of elements that aim to improve connectivity and activity throughout the BIA.
- This master plan is prioritizing several smaller gathering and meeting places throughout the BIA, as opposed to one central square.
- Some of the sites highlighted are based on possible partnerships with

current and future land/building owners throughout the BIA.

- Partnerships with landowners should be encouraged to promote mural art, tree planting and soft landscaping along private property adjacent to the public realm.
- The BIA supports improvements to building façade finishes through both the Façade Grant and Mural Grant. The aim of these grants is to enhance and animate the sidewalks in Weston Village.

All new development within Weston should consider POP's (privately owned public space), increasing parkland dedication and highlighting the BIA's adjacency to the Humber River. Acquiring land and strengthening the connection to the Humber River should be prioritized. Low-rise residential should be discouraged along Weston Rd. as it breaks up the commercial pattern and consistency of the main street.



Mapping of the master plan with annotated project descriptions

# ENHANCE CONNECTIVITY - WESTON RD. PUBLIC SPACE

As recommended by the City of Toronto Retail Design Manual, the following considerations to be made along Weston Rd.:

- New developments should consider large building setbacks, side street residential entries and retail uses at the ground floor with vibrant interiors and an active outdoor presence.
- Promote retail tenants along Weston Rd. to create a continuous retail street. This should also include expanded food offerings as noted in the ULI TAPS (2018) report.
- Locate public parking below grade where possible in new developments.
- New retail development should consider high quality materials, and detailing and environmentally sustainable materials.
- New retail development should

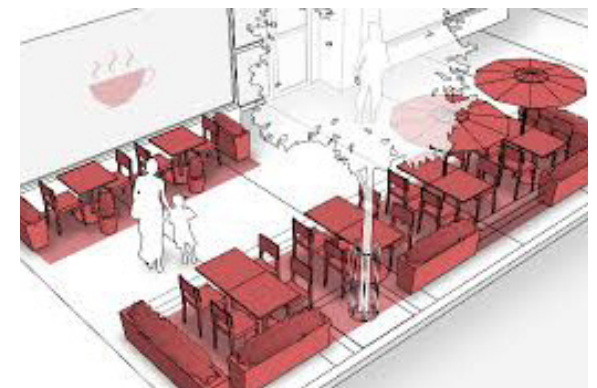
accommodate additional open space through building setbacks that offer public amenities such as trees, seating, pedestrian lighting and public art.

- Avoid unanimated blank walls on all street frontages.
- Maximize the retail window display along Weston Rd. that showcases merchandise and clear views into the retail space.
- Promote the use of operational window systems (for example garage doors) to make a stronger connection between the business and sidewalk.
- Ensure vibrant lighting within the retail space during the day and reduced illumination at night after retail hours to support night-time activation of the retail street.

**In order to implement curbside cafe's and retail spill out space along Weston Rd. the pedestrian clearway would need to expand and the number of lighting, hydro and TTC poles/elements would need to be reduced. Sidewalks that have not been included in the most recent street construction along Weston Rd. fall short of the 2.1 m pedestrian clearway recommendation (John Street Revitalization Report), and should be labeled a priority for future widening and/or replacement - as noted in the PSCC Study (2018).**



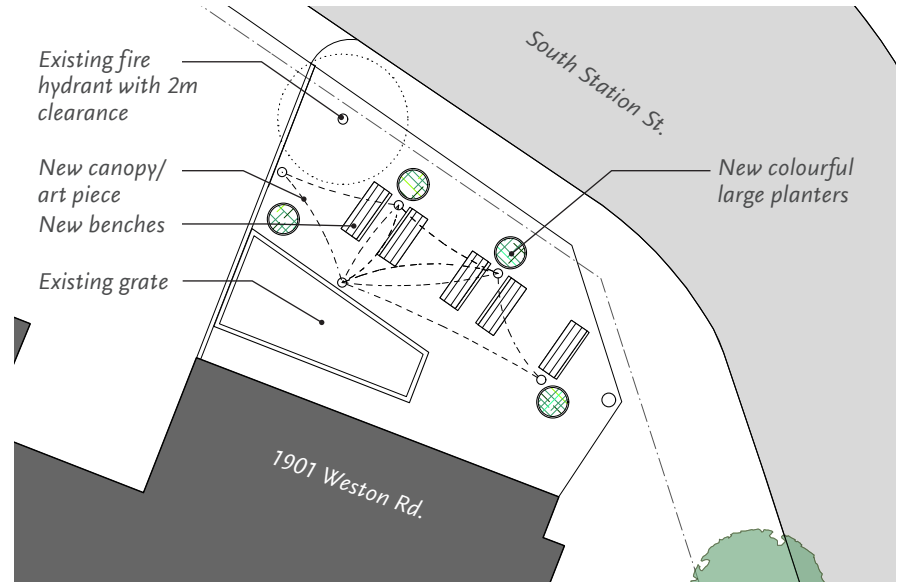
Diagram illustrating public and tenant amenities from *The City of Toronto Retail Design Manual (2019)*



Typical curbside café configuration from the *City of Toronto Sidewalk Café Manual (2015)*,



Curbside cafe space in King West, Toronto



Proposed Parkette on South Station St.



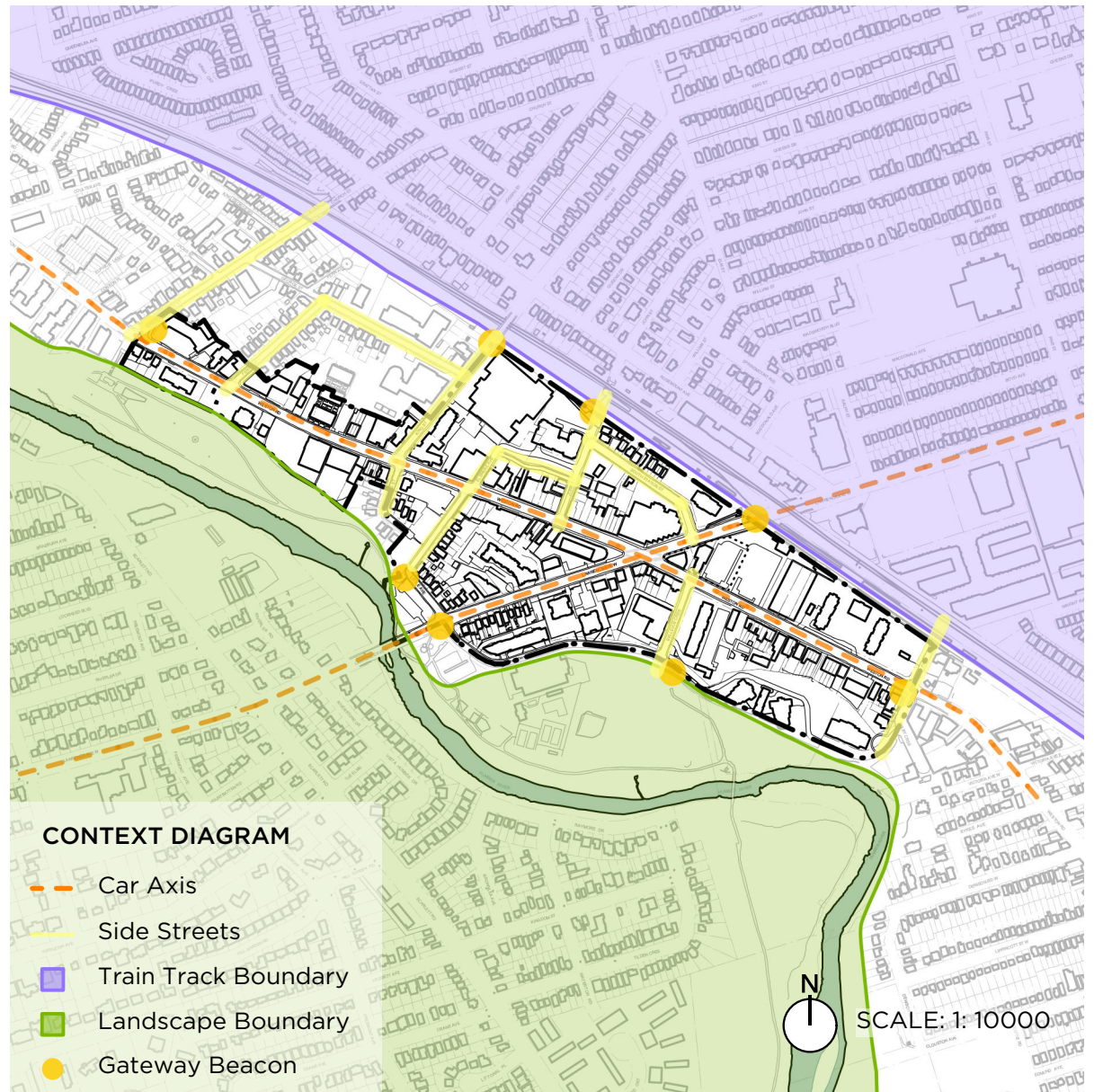
Pedestrian clearway Downtown Bordentown, New Jersey. Photo courtesy of JGSC Group.



Main St. in Greenville, South Carolina

# ENHANCE CONNECTIVITY - SIDE STREETS

- Weston Rd. is not coherent and it will be difficult to link all of Weston through the centre spine, therefore the cross streets need to be activated.
- Promote active crossings throughout the entire length of the BIA to link the east and west sides of Weston Rd.
- Highlight the laneways and secondary streets as a primary pedestrian and cycle network throughout Weston Village as noted in the Weston 2021 (2012) report.
- Secondary streets such as Fern Ave., King St., Elsmere Ave., Pantelis Kalamaris Ln., Little Ave., John St., South Station St., Hickory Tree Rd., Bellevue Cres., Wright Ave. and Wilby Cres. have ROW widths which allow for sidewalk expansion and bump outs - room for growth for vegetation and pedestrian interaction.
- Refer to the John Street Revitalization Public Realm Improvement Plan by the Urban Land Institute and Dtah for side street sections and additional activation strategies.
- All future work should aim to improve lighting, pedestrian connection, park linkages, retail improvement and lane way revitalization.
- Any opportunity for additional public parking on side streets should be considered.

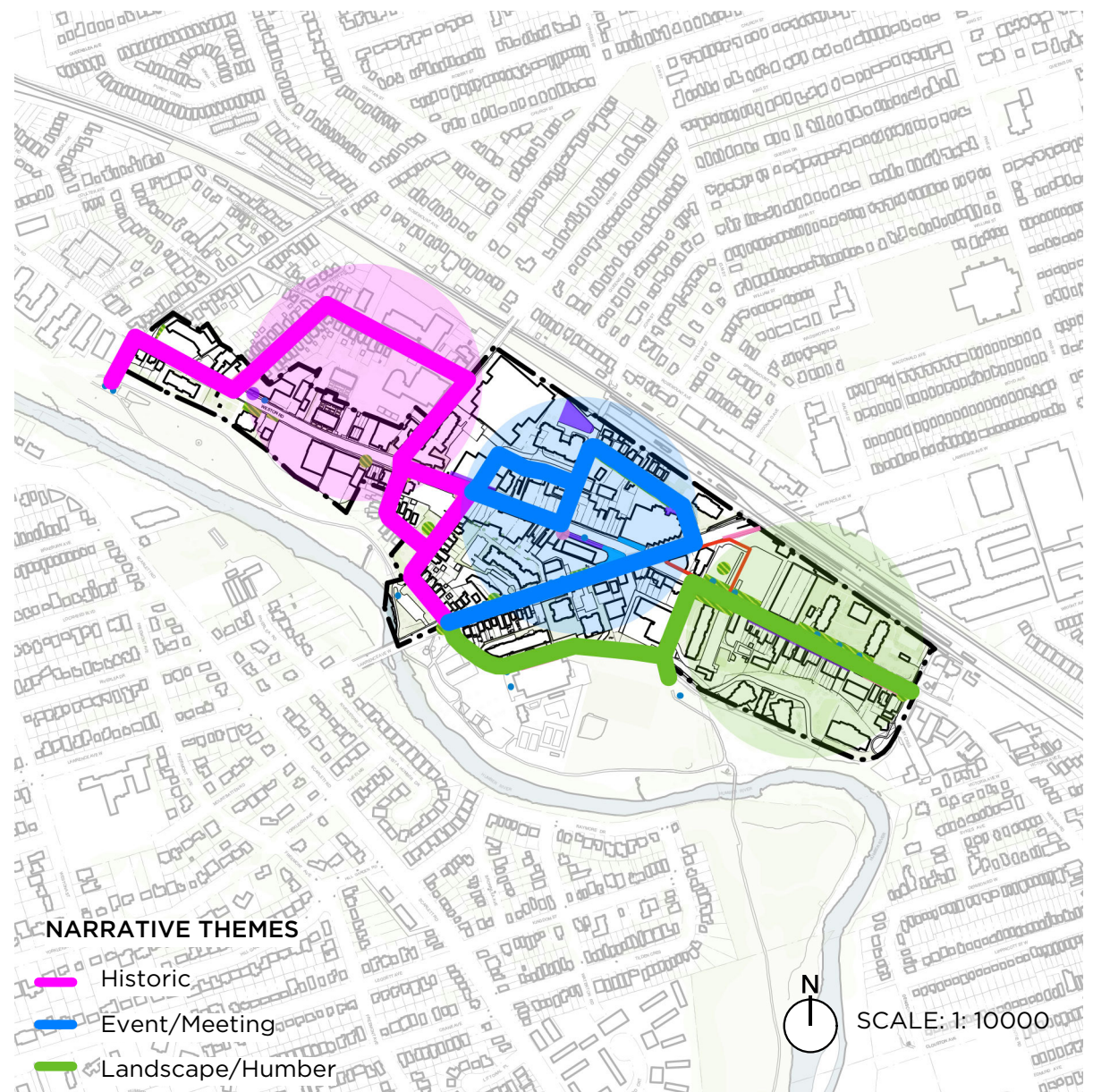


Mapping of proposed side street connectivity



# ENHANCE CONNECTIVITY - THEME WALKS

- To enhance the side street activation, three thematic zones have been identified to help link the treatment along Weston with the side streets. These could be used to make walking tours, to thematically govern artworks/ murals and landscape elements.
- The Historic zone is centred around important historic buildings and connects the Weston residential area to the north. This zone can help to tell the story of Historic Weston Village.
- The Event/Meeting zone connects the new John St. meeting areas to the central Weston Rd./Lawrence Ave. W. crossing, and the future site at the south east that will have a publicly accessible open space.
- The Landscape/Humber zone includes a key access point to the Humber River multi-use trail at the south. The area along Weston Rd. contains the most amount of existing soft landscaping, and trees, and can tell the story of the Humber Adjacency.
- The heritage and Humber themes can be used to promote educational visits to Weston, as recommended by the ULI TAPS (2018) report.



Mapping of proposed thematic walks

# ENHANCE CONNECTIVITY - BEACONS

- Gateway locations have been identified for pedestrians, cyclists and cars to mark entry, as well as promote a strong cross and side street pedestrian system. The gateways will stand as identifiable landmarks as recommended in the John Street Revitalization Report (2013).
- New vertical gateway markers are proposed. The gateways should celebrate the BIA penny farthing symbol AND the green connection to the Humber River, including the wide bend in the river – promoting Weston as a riverfront community as noted in the ULI TAPS (2018) report.
- Future gateway and art design should aim to enhance the existing BIA branding including material and colour (refer to BIA Branding and Furniture).
- At Lawrence Ave. W., the gateway could be a mural treatment, incorporating an early Weston sign greeting that once read: Come Again...And Stay!
- All gateway projects along the southwest edge of the BIA should be completed as a partnership with the BIA and Parks, Forestry and Recreation, and should consider more enhanced gateway concepts that include stopping and gathering areas.



*Painted rail overpass in Toronto*



*Kew Gardens by PLANT Architect Inc.*



*Roosevelt Rd. Gateway Concept by Site Design Group Ltd.*



*Shanghai Houtan Park by Turenscape*



*Historic Gateway for Weston Village - Image from Weston Historical Society*



*Serpentine Pavilion by SANAA*



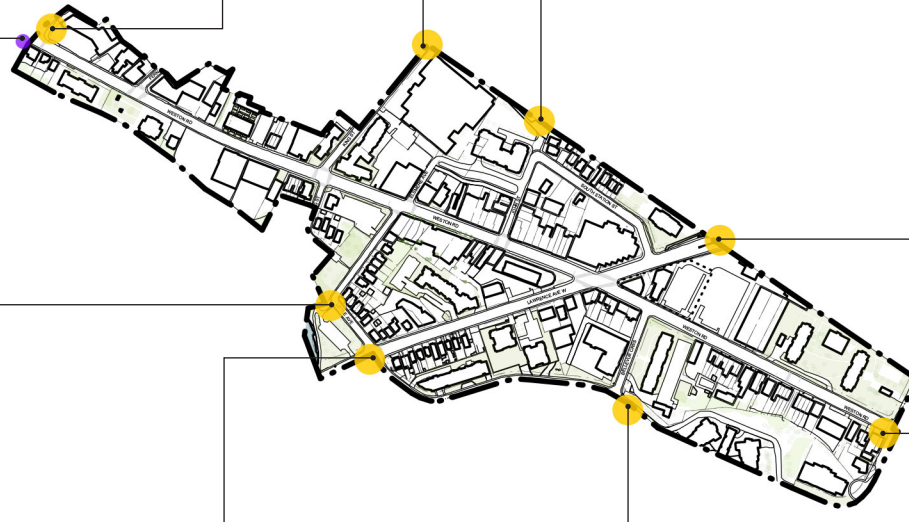
2



3



1+3



1+3



2



1+3



1+3



1+2+3



1+2+3

LEGEND

- 1. Consistency throughout gateway entries.
- 2. Refurbish existing gateways.
- 3. Create artful welcome and accessible entries.



SCALE: 1: 10000

Mapping and images of existing gateway locations



East Gateway on Lawrence Ave. W.

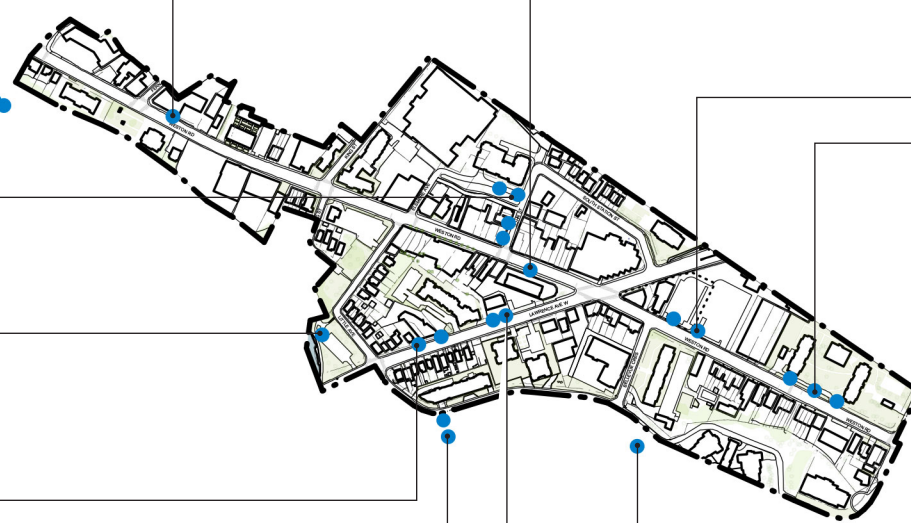
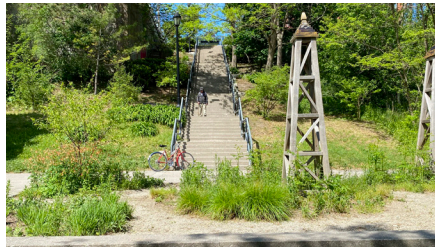


South Gateway at Weston Rd. and Wilby Cres.



West Gateway at Bellevue Cres.

# ENHANCE CONNECTIVITY - BIKE PARKING



- In order to increase bike transportation throughout the BIA there needs to be available bike parking. Install bike parking in areas that are lacking.
- Bike parking along the Humber River multi-use trail to be completed by the City of Toronto.
- Bike parking should be implemented using the penny-farthing post and ring designed specifically for the BIA.
- Six Bike Share stations are being proposed to promote inter modal connectivity as noted in the PCC Study (2018).
- Add bike parking in orphan spaces at the TPA parking lot on John St.

N  
SCALE: 1: 10000

Mapping and images of new bike parking locations

Penny-farthing bike rack (designed/manufactured by Artist Scott Eunson)

# ENHANCE CONNECTIVITY - HARDSCAPE

- While the intersection of Weston Rd. and Lawrence Ave. W. is the area's core, there are many other important small nodes scattered throughout the BIA. Benches and planters should be replaced and planting maintained in these areas.
- Meeting areas to remain small and conversational as opposed to areas for events.
- Areas that are currently empty and could use additional seating have been identified in the adjacent mapping.
- Benches have been selected by the BIA for the new work at Weston Rd. and Lawrence Ave. W. This bench should be repeated for the rest of the BIA. Refer to the BIA Branding and Furniture chapter for model and colour specifications. Planter colours could change along the street progressing from one end to the other.
- The master plan identifies all side streets that have a ROW width of 6m or more. Therefore, these sidewalks can be widened and normalized, and/or bump-outs can be added.
- A pedestrian clearway of 2.1 m to be provided on all sidewalks. All obstructions should consider individuals with physical challenges.

Ensure AODA compliance with sidewalk clearances for any proposed obstruction within the ROW.

- Replace raised tree planters with in-ground trees in the furnishing zone. This will bring opportunities to support vibrant main streets with a larger pedestrian clearway. The City should continue to explore other opportunities for green street solutions within the BIA. Permeable pavers are recommended as oppose to concrete whenever there is tree planting, hardscape close to parks and hardscape abutting any new development.
- Repeat the use of the City-standard grey pavers for any future work along the side streets within Weston Village. Currently these pavers are used along John Street and should be incorporated into South Station St. and Elsmere Ave.
- Future sidewalk widening along Weston Rd. should be implemented with the use of concrete as oppose to pavers.
- Although the BIA identified the need for hard scape improvements at Cruickshank Park, the park is beyond the BIA boundary, therefore any work to this entry should be completed by the City.



*Illustration of Eglinton streetscape (Eglinton Connects)*

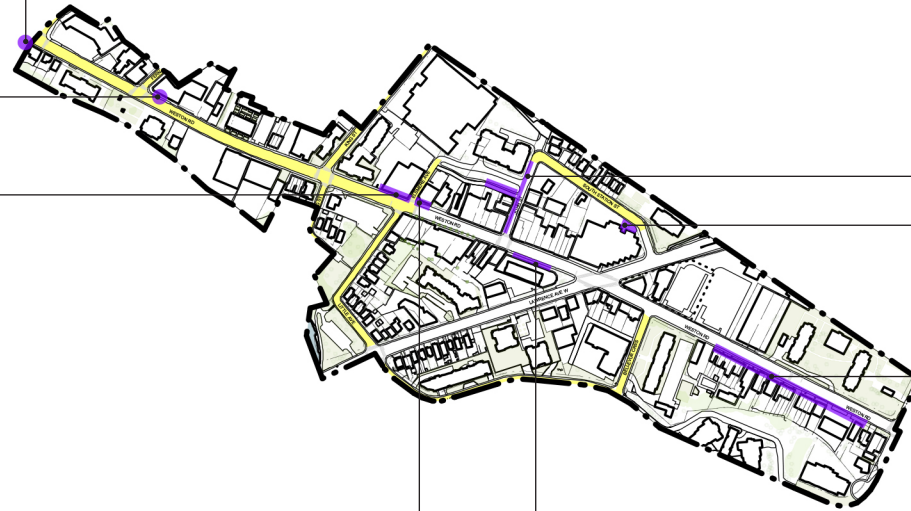


*Portland Green Streets curb bump-out*



*Curb bump-out image from the Toronto Entertainment District Streetscape Manual*





LEGEND

- 1. Replace existing benches.
- 2. Add surface treatment to existing planters and other miscellaneous hardscape elements.
- 3. Increase safety near bus stops through lighting.
- 4. Promote the pedestrian road at John St. for possible events and intermittent street closure and strengthen partnership with Artscape.
- 5. Refurbish lane.
- 6. Add softscape/trees.
- 7. Replace planters with temporary large planters.



Sidewalk widening to be incorporated into future City of Toronto plans.



Mapping and images of hardscape revitalization locations

# LANDSCAPE REVITALIZATION

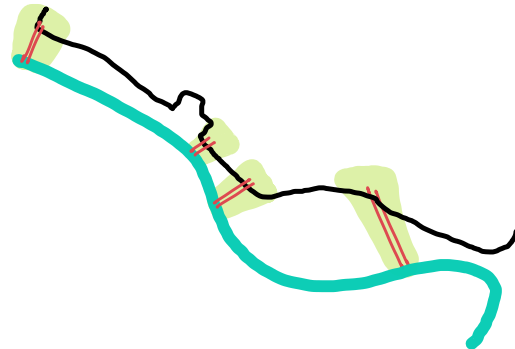
- The open space network should be considered as an ecologically diverse network with priority given to native planting, pollinating and bird-attracting species, and demonstrative/educational opportunities.
- Tree planting on private property adjacent to the public realm is encouraged.
- Promote Weston Village as a sustainable neighbourhood.

The adjacency to the Humber River should be emphasized. Opportunities include:

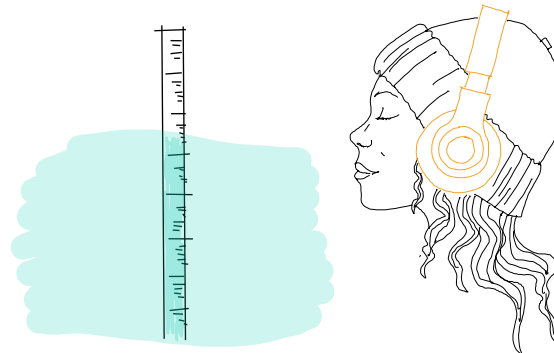
- Humber River history;
- Humber River ecology;
- Sounds of the Humber River
- Untold stories of the Humber River;
- Humber River access;
- The future of the Humber River
- Landscape elements with informative/educational notes/images; and
- Promote Weston Village as a sustainable neighbourhood for educational field trips.



*New Street Trees Throughout the BIA with Labels*



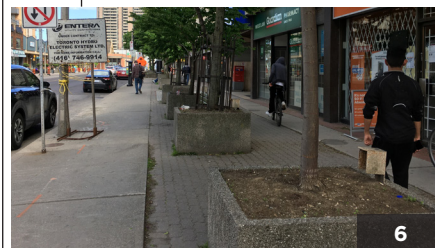
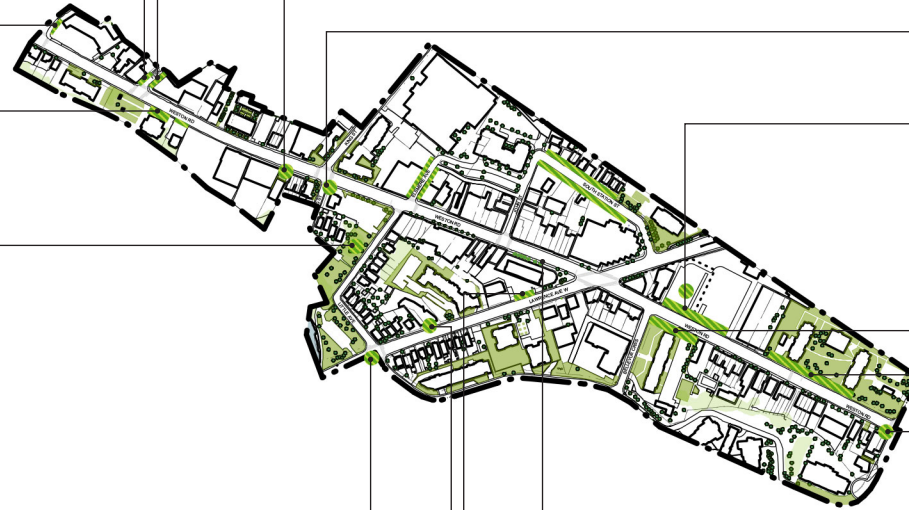
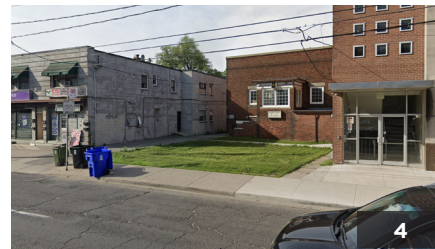
*Highlighting Connections from the River to the Table Land*



*Physical Markers and Sound Installations Educating the Public on the Humber River Adjacency*

## LEGEND

1. Redevelop new and existing wide sidewalks to include planting and new or refurbished tree canopy.
2. Revitalize Little Avenue Memorial Park. The park was refurbished in the early 2000's but requires maintenance and updating.
3. Partner with developments that have neglecting the vegetation in front of their property.
4. Partner with Masonic Temple to convert passive green space into active planing and/or tree canopy.
5. Increase the health of existing street trees by providing more effective softscaping below. Existing tree surrounds located at the south end of the BIA (along Weston Rd.) should be removed to improve the future health of the trees.
6. Add surface treatment to existing planters.



SCALE: 1: 10000

Mapping and images of landscape revitalization locations

# LANDSCAPE REVITALIZATION

Where new soft landscaping is proposed:

- Promote trees as much as possible, especially along secondary roads throughout the BIA. Coordinate tree species selection with Urban Forestry and maintain minimum soil requirements. Tree planting to conform with *Tree Planting in Hard Surfaces* guidelines and provide Type T-CIP-3.0 where possible.
- Planting to be in ground with metal barriers to discourage walking through and in hangers and temporary planters to allow the BIA flexibility to move them around. Where pedestrian clearways are tight, trees to be provided in tree grates to maximize the sidewalk space Type T-CIP 5.0 or 6.0.
- Maximize more temporary planting away from seating to minimize litter.
- Proposed planting to include annuals, perennials, grasses and shrubs. Annuals bring early colour in the spring, and grasses, perennials and shrubs help to improve the ecological environment.
- Planting species must be robust and accompanied by a strong maintenance regime and budget.



*Vegetation out of harm's way/above grade*



*Perforated fence surround Type T-CIP-3.0*



*Large temporary planters*



*Tree with seasonal change of colour.*



*Tree with seasonal change of colour.*



*Tree in grate*



*Perennial*



*Perennial*



*Perennial*



*Perennial*



*Grass*



*Grass*



*Evergreen*



*Evergreen*



*Evergreen*

Fern Ave. provides an opportunity for planting trees and a mural on both sides of the street.



Parkette planting and mural at Fern Ave. and Weston Rd.

The north gateway at Church provides an extensive area in the right of way for landscaping which can mitigate the stark openness of this corner.



North gateway with new beacon and planting at Church St. and Weston Rd.

# ART, WAY-FINDING AND LANDMARKS

- Murals and landmark art pieces to be used as way-finding to direct people toward significant areas both on and off of Weston Rd.
- Existing landmarks like Memorial Park need some pointing to. A sign at the northwest corner of Weston Rd. and Little Ave. is proposed to guide visitors.
- There are several walls that are available for murals which have been identified in the adjacent plan.
- The proposed mural at Weston Lions Arena to be completed in partnership with the BIA and Weston Lions Park and Arena.
- Vertical markers are proposed for art and signage to provide more obvious landmarks given the heterogeneity of the background.
- Weston and Humber River histories and present day stories provide possible themes for future artworks.
- Future art throughout the BIA to consider the subsequent thematic walking areas and aim to enhance them.
- Temporary cladding of the concrete tree planters on Weston Rd. could be an art commission.
- New penny-farthing pole ornaments to be installed on all applicable poles throughout the BIA and replace existing.
- All new developments to include public art to promote Weston as an

arts hub - as noted in the ULI TAPS (2018) report.

- ArtworxTO and The Public Art Strategy should be considered prior to the award and selection of public art works throughout the BIA.
- A seasonal art piece is proposed for the Artscape Plaza at 22 John Street. The new art piece should be demountable for storage and enhance seasonal changes and annual events.



Vertical art by Pierre Vivant



Vertical art by Susan Schmögnner



Existing penny-farthing pole ornament

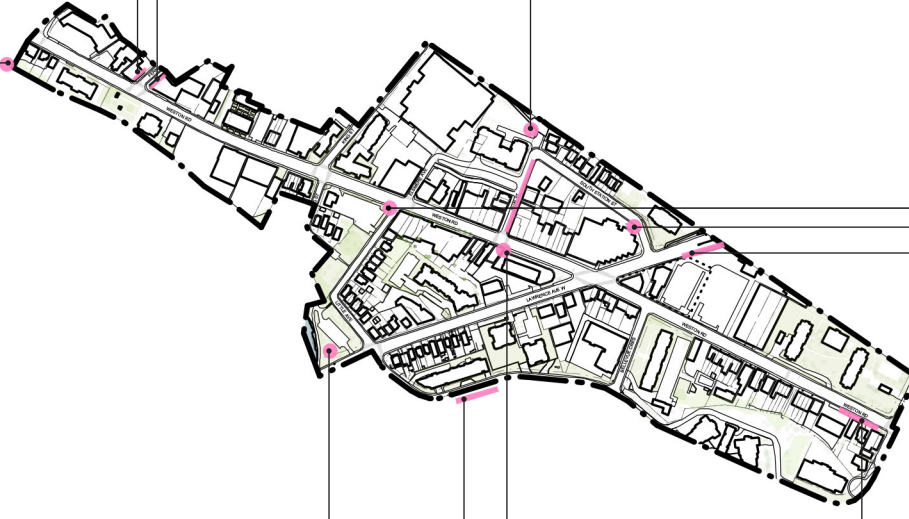


Mural by Philip Coté



Mural by Nick Sweetman



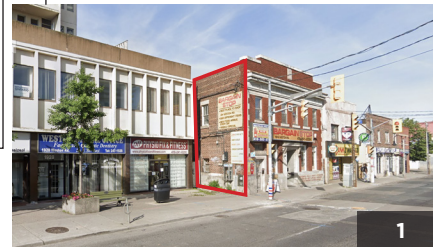


LEGEND

1. Use existing hard surfaces for future mural treatment.
2. Increase visibility of hidden BIA assets such as Little Avenue Memorial Park and access to the Humber River through creative way-finding from Weston Rd.
3. Additional art/sculptural pieces.



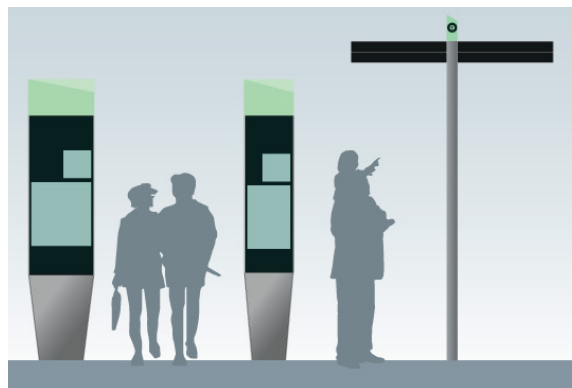
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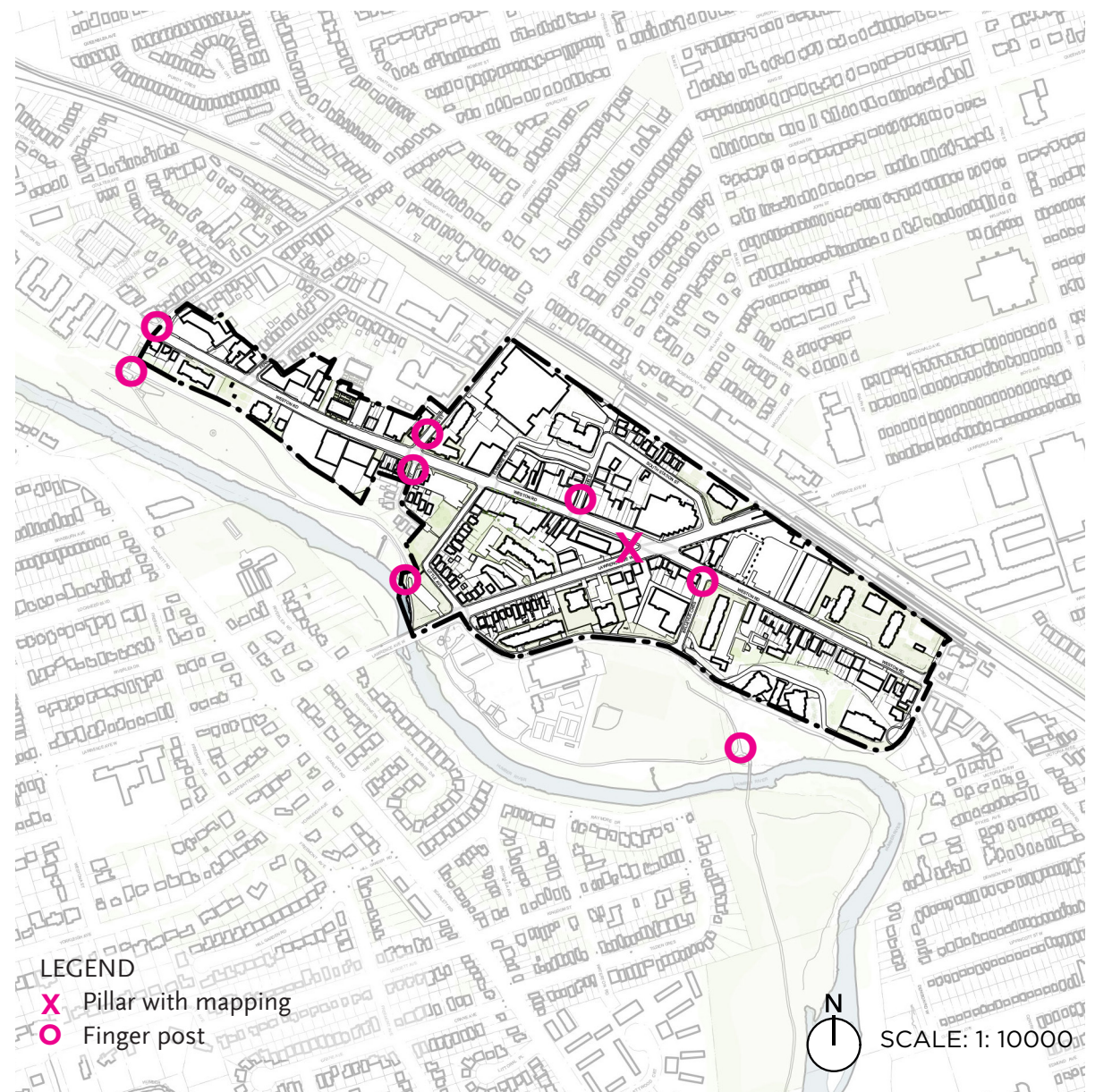
Mapping and images of proposed art, way-finding and landmark locations

# ART, WAY-FINDING AND LANDMARKS

- Future way-finding development to coordinate with the TO360 Way-finding Program. Recommended products include:
  - TO360 wide way-finding pillar,
  - TO360 narrow way-finding pillar,
  - TO360 way-finding finger post
- TO360 wide (or narrow) way-finding pillar to be installed at a central area in the BIA such as at the corner of Weston Rd. and Lawrence Ave.
- The TO360 way-finding finger posts to be installed along Weston Rd. and the Humber River multi-use trail to notify pedestrians of side street activities and connections between the river and the tableland.
- Final placement to be determined on-site with Transportation Services (TO360). Wayfinding locations to be finalized so as not to impede the pedestrian clearway or block site lines.



Recommended TO360 Way-finding products



LEGEND  
 X Pillar with mapping  
 O Finger post

N  
 SCALE: 1: 10000

Mapping of recommended locations for future Toronto TO360 Way-finding signs

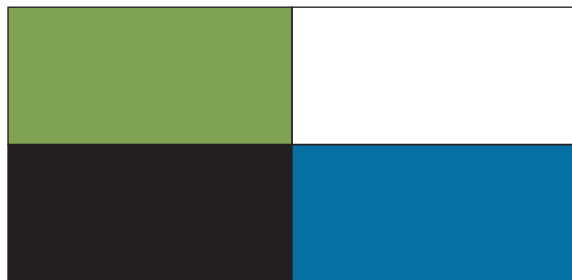


Way-finding to Little Memorial Park

# BIA BRANDING AND FURNITURE

- Future branding throughout the BIA should incorporate a mid-tone blue to highlight the Humber River adjacency in addition to the existing olive green, black and white branding colours.
- New steel elements should be considered in weathering steel or black powder coated steel with the addition of a green or blue accent.
- All benches to be replaced with the Classic Displays (Olive) bench.
- All existing concrete planters to be replaced with new self-watering planters, except for those adjacent to 1906-1930 Weston Rd., which currently contain trees. New planters to engage the BIA colour palette.
- New pedestrian lighting poles to be black in aggregate concrete with hardware to accommodate one hanging basket and one 4 foot banner.
- Promote Weston Village as the “Home of the Bicycle”.

Refer to Appendix D for bench, pedestrian lighting, clock relocation and planter specifications.



BIA colour palette



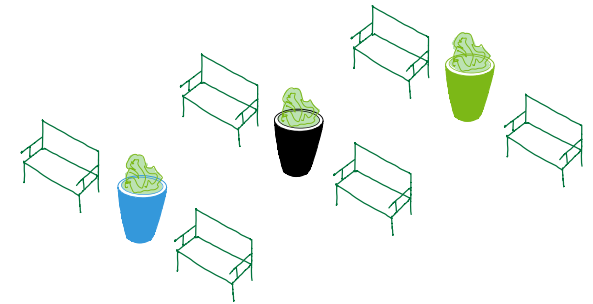
BIA banners



BIA logo on bench



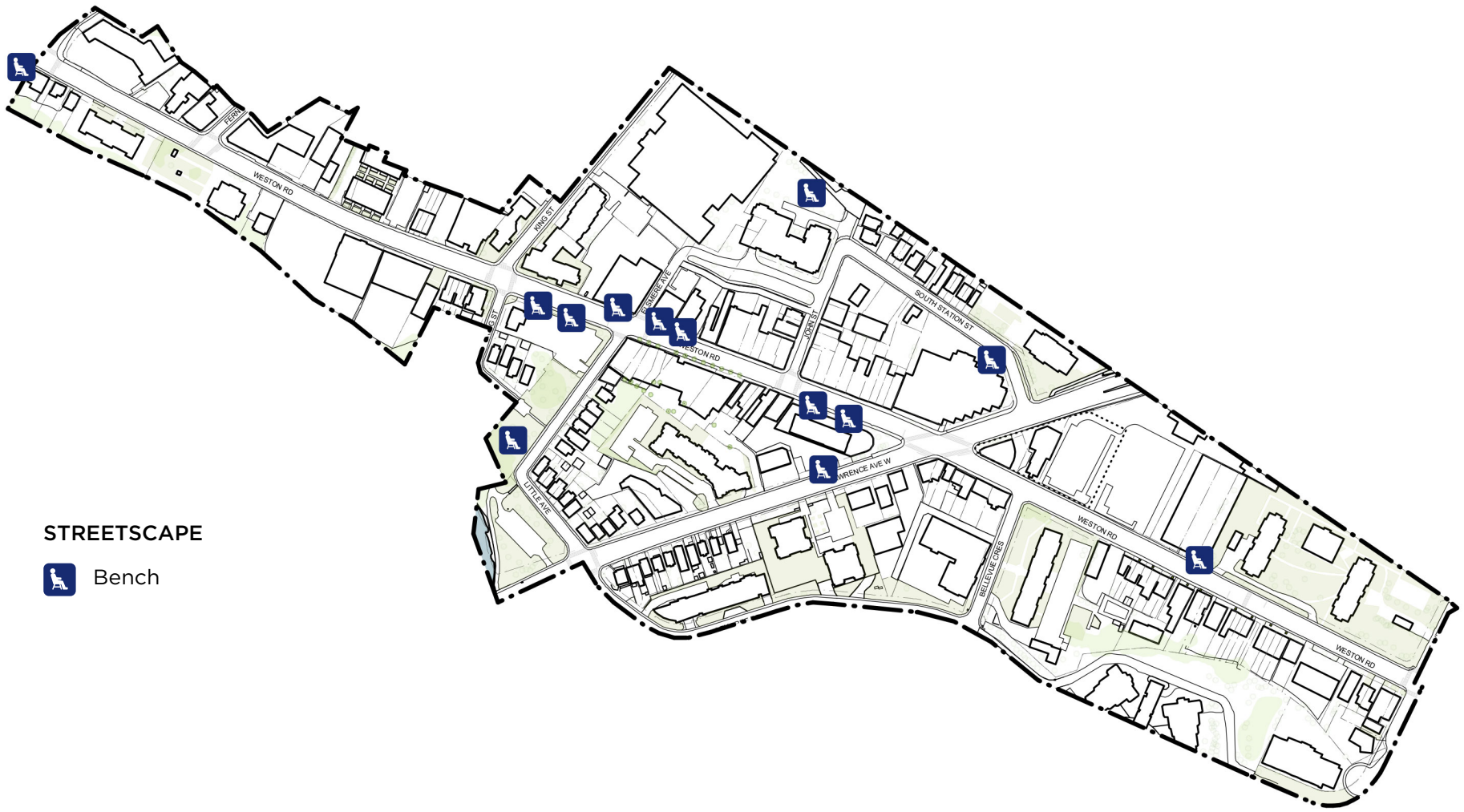
BIA bench by Classic Displays (Olive)




Multi-coloured planters



Terrace 42 Planter by Desert Planters



**STREETSCAPE**

 Bench

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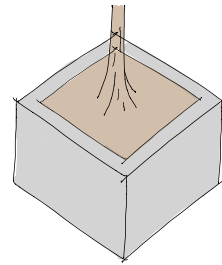
*Mapping of proposed seating areas and areas where benches are being replaced*

# BIA BRANDING AND FURNITURE

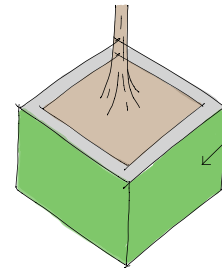
There are eight existing planters at 1920 Weston Rd. which contain trees in fair/good condition. These concrete planters are in poor condition. When these trees reach the end of their life, they should be replaced with new trees in grates. The cladding proposed here is temporary and does not require any physical attachment to the existing planter.



Penny-farthing bike rack constructed out of weathering steel (designed/manufactured by Artist Scott Eunson)

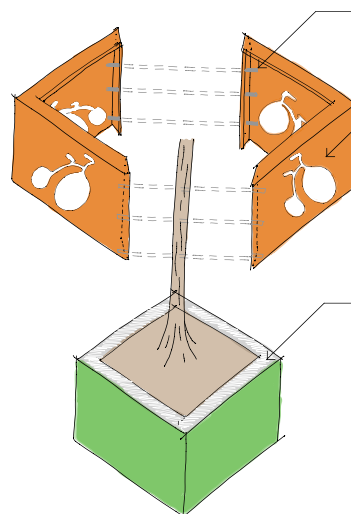


1. EXISTING PLANTER



2. EXISTING PLANTER PAINTED

PAINT SIDES OF EXISTING CONCRETE PLANTER. COLOUR TBD

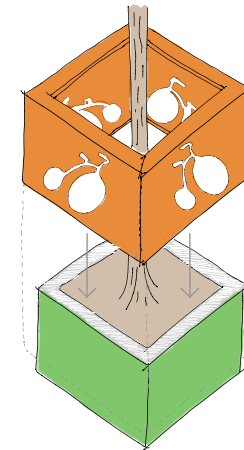


3. CORTEN ASSEMBLY

BOLTS

LASER CUT CORTEN (2 PIECES), BOLTED TOGETHER ON SITE AROUND EXISTING TREE

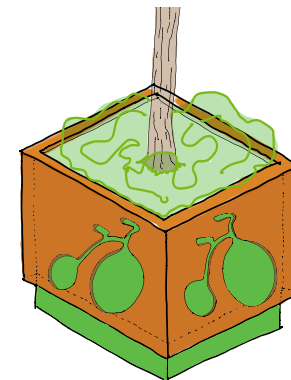
GLUE APPLIED ON TOP EDGE OF EXISTING CONCRETE PLANTER



4. CORTEN CLADDING INSTALLATION ONTO EXISTING PLANTER

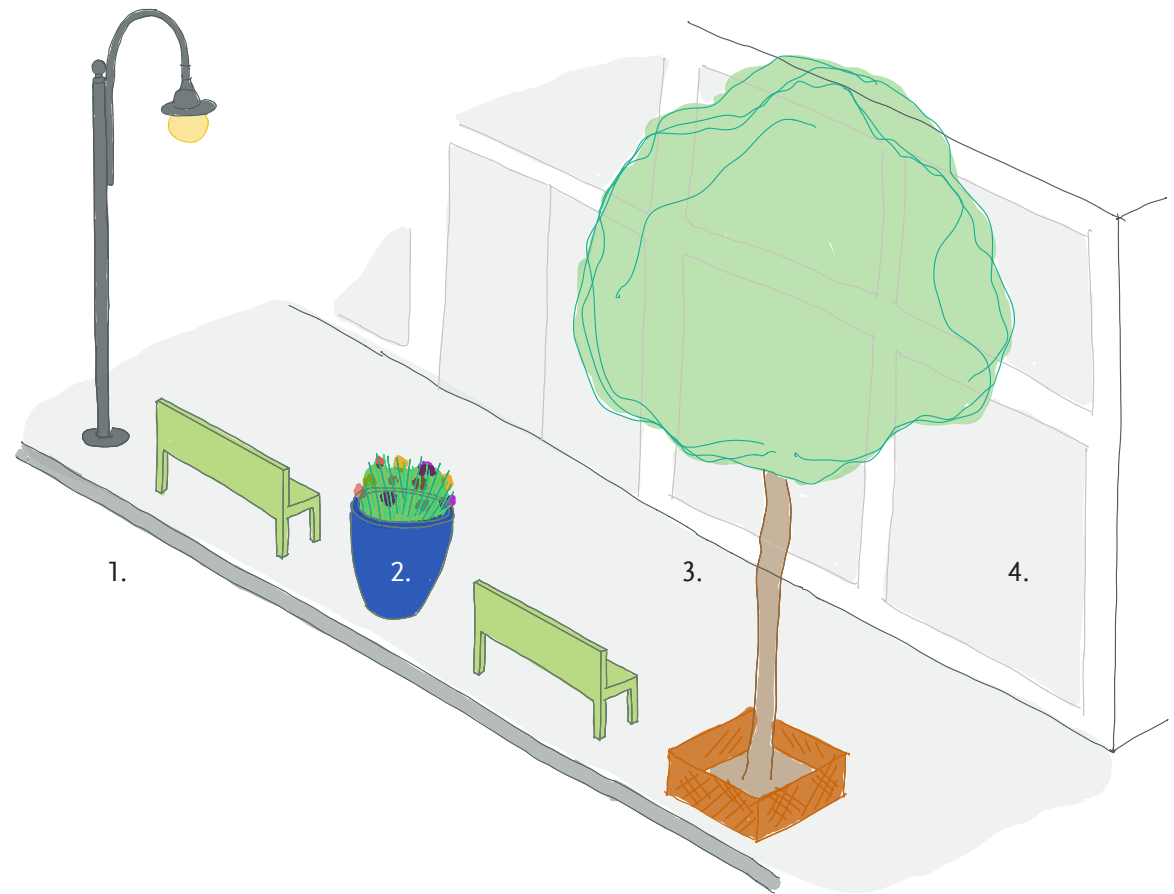
LOWER CORTEN CLADDING ONTO EXISTING PLANTER. NO FASTENERS INTO PLANTER

Corten cladding assembly on existing concrete planter at 1920 Weston Rd.



New corten cladding over existing painted planter boxes

Garbage has been a continuing problem within the Weston BIA, with cans overflowing. Additional receptacles are recommended to ensure one receptacle at each corner – two on each block throughout the BIA. In instances where the block is longer than 125m in length, additional mid-block receptacles are recommended.



- LEGEND
- 1. Street
  - 2. Benches/planting/lighting all facing building
  - 3. Pedestrian clearway
  - 4. Building



City of Toronto waste bin by Astral Media

General locations for BIA furniture

# HISTORIC NARRATIVES

There are many historic narratives in Weston that can be drawn upon:

- Honour the Carrying Place Trail and Indigenous history of the Wendat peoples dating back 600 years.
  - Identify the planting that used to exist in the area - a saw mill was centred in Weston based on it's impressive supply of oak and pine bush.
  - Acknowledge past significant industrial sites (CCM & Kodak).
  - History of Weston as a self-contained hamlet (known as "The Humber"), a village, then a town before becoming part of the Borough of York, then eventually the City of Toronto.
  - The BIA's existing artifacts could be used/displayed or referenced in a central gathering area in the BIA for education and reflection - ideally in the heritage themed area.
- Embrace the heritage and identity of the area by integrating identity markers, and heritage commemorations in the overall streetscape as mentioned in the ULI TAPS (2018) report.
  - Honour the manufacturing past by highlighting materials and fabrication – this idea aims to highlight how many different industries (and small shops) in Weston turned raw material into useful goods. It is not nostalgia for specific lost companies (Kodak and CCM).



*Stone*



*Drift wood*



*Manufactured steel*

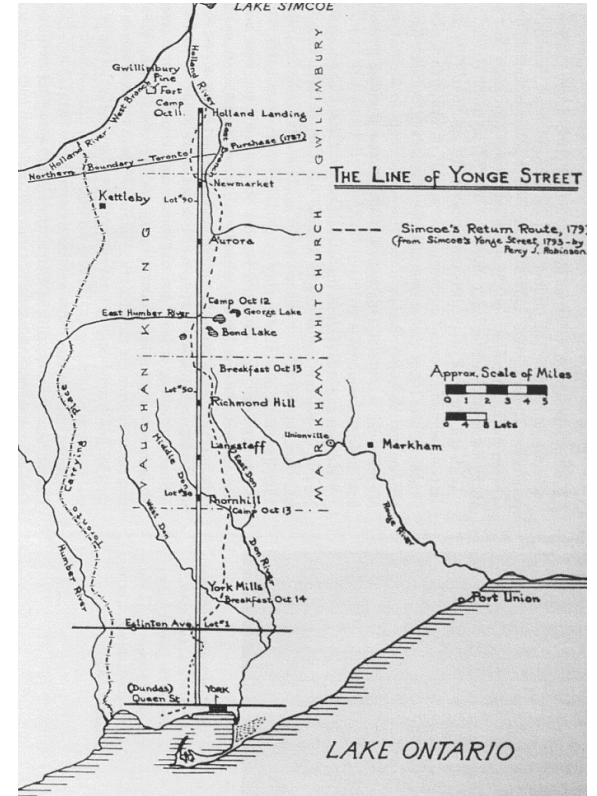




Image from Weston Historical Society



Image from Weston Historical Society



Carrying Place Trail Map



Image from Weston Historical Society



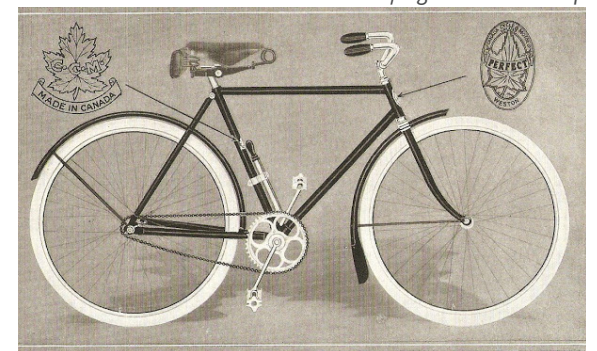
Image from Weston Historical Society



Image from Weston Historical Society



Image from Weston Historical Society



MODEL 656 PERFECT ROADSTER

CCM bike

# HISTORIC NARRATIVES - WALK

This master plan aims to give Weston a historical address using a series of artifacts that connect the historic civic centre with Little Avenue Memorial Park. The historical elements will work to enhance the green boulevard bounding the current funeral home parking lot, which previously held Weston's town hall (Dufferin Hall).



*Weston Historical Society Plaque*



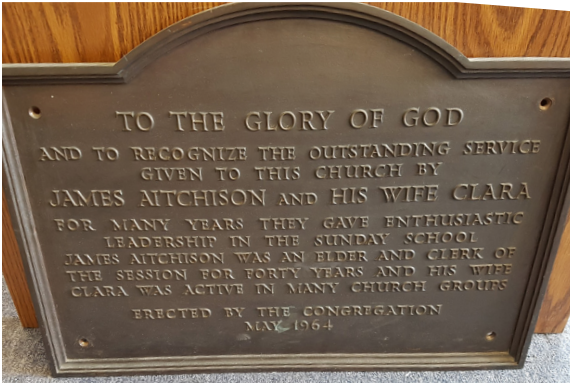
*'Wadsworth Arch 1910' plaque*



*'Dufferin Hall' stone carving*



*Plaque*



*Plaque*



*'Odd Fellows Welcome' sign*



**HISTORIC COMMEMORATION**

- Heritage | Designated
- Heritage | District
- Heritage | Listed
- Heritage | Study Area
- Notable Historical/  
Landmark Buildings
- Heritage | Walls
- Heritage | Toronto  
Carrying Place Trail
- Historic Narrative Walk  
with Artifacts
- Historic Zone

N  
SCALE: 1: 5000

Mapping of the proposed historic commemorative walking route

# COSTING AND PHASING

## BUDGET SUMMARY

The chart on the following pages outlines short, mid, and long term projects and programs based on the work described in the master plan. Costing is based on a quantity survey prepared by A.W. Hooker, which is attached as Appendix A: Cost Estimate. For each scope of work, the costing summary indicates whether architectural/engineering services are required, and includes contingencies for design and construction. As the exact year of implementation for each item has not been determined, some escalation costs will apply. Escalation rates per annum are typically 3.25%. Some works may also require traffic studies (boulevard curb bump-outs, sidewalk reconstruction along the Weston Rd.).

These costs are considered 'Class D' based on conceptual/schematic designs, and therefore are to be used for coarse budgeting and determination of priorities. More refined budgets would be developed for each master plan component project as the final design and implementation of these projects is developed.

All costs shown are in total and do not take into account cost-sharing with the City or other partners. In general, the BIA cost-shares with the City for permanent works. For the public park projects, it would be expected that the

Parks, Forestry, & Recreation department would contribute funds, however this will require negotiations once the department is brought on board with the BIA's intentions for these areas. Maintenance of all bespoke or custom street furniture, planting in the right-of-way, and other BIA-commissioned streetscape elements will be the responsibility of the BIA. The Urban Forestry department is responsible for all public street trees.

## PRIORITIES AND IMPLEMENTATION

Some of the work proposed will take many years to implement, and some can be initiated quickly. A number of critical path elements require the City to provide input, studies, or coordination in order for some elements to move forward. The priorities listed indicate critical coordination that is needed early in the implementation, followed by priorities as set out by the BIA.

The primary focus should be on replacing the existing benches; removing existing concrete planters and replacing them with large colourful planters, wayfinding and signaling to adjacent parks and the Humber River, vibrant murals, the historic walk and implementation of the gateway beacons, as these are moves that can be deployed throughout the BIA to bring

consistency and cohesion to the disbursed parts.

Some of the projects proposed, such as the publicly accessible open space at 1885 and 1871 Weston Rd., propose partnerships with new and existing private entities within the BIA. Definitive proposals for these sites should be developed by the Weston Village BIA in conjunction with property owners.

Medium and large-sized projects will have a longer time frame for implementation due to consultations required with City departments and other stakeholders. The gateway interventions that aim to promote a shared language upon entering and exiting the BIA should be prioritized.

Sidewalk widening and curb bump-outs should be prioritized throughout the BIA to promote pedestrian movement and room for tree planting within the ROW. These projects form part of the long-term implementation strategy and should be pursued in conjunction with planned City capital works, implementation of the Complete Streets strategy for the Weston Village, and property development.

In general, the master plan should be reviewed/refreshed on a minimum 5 year basis, with the BIA reviewing the priorities yearly. The total extent of implementation will require a

number of years to complete, and parts of this master plan will need to be revisited during the process. We therefore recommend a review/refresh in early 2026.

General Right-of-Way requirements must be adhered to in the implementation of the master plan, and will be considered in detail in the respective design phases for the proposed interventions. Each project will be vetted by the City of Toronto Transportation Services / Public Realm department and subject to the following requirements:

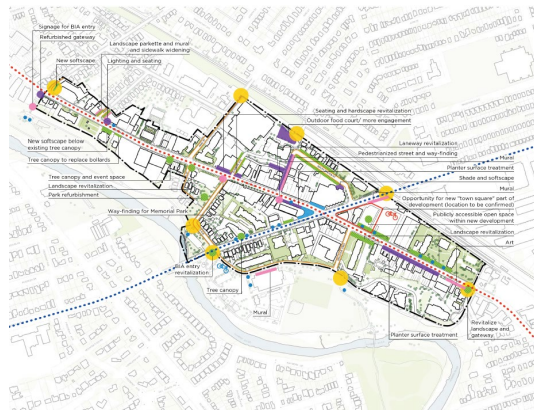
- Adherence to all minimum setbacks;
- Conformance with all height restrictions (including vegetation);
- Preservation of sight lines: Projects must not create or contribute to sight line obstructions;
- Approval/sign-off by all required Utilities;
- Maintenance agreement(s);
- All seasonal installations to be removed during winter maintenance season, where applicable;
- Preservation of access/egress to private property, where applicable; and
- Adherence to AODA requirements

# APPENDIX A: COST ESTIMATE

Weston Village BIA  
Streetscape Improvements

## Master Plan

Class D Estimate (Rev.2)



Prepared for:  
PLANT Architect Inc.

Prepared by:

**A.W. HOOKER**®  
QUANTITY SURVEYORS

2265 Upper Middle Rd. E Suite 400  
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January 14, 2021

120179, Weston Village BIA Streetscape Improvements

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THE PEOPLE | THE DIFFERENCE™

January 14, 2021

PLANT Architect Inc.  
Suite 208 – 101 Spadina Avenue  
Toronto, ON  
M5V 2K2

Attn: Taylor Gould,

Re: Weston Village BIA Streetscape Improvements, Class D Estimate (Rev.2)

Dear Taylor,

Please find enclosed our Class D Estimate (Rev.2) for the above project. The estimate is based on design drawings and information provided by Plant Architect Inc. received on November 16, 2020.

This version of the report incorporates where necessary the comments provided by Plant Architect Inc. received on January 04, 2021 through January 12, 2021.

This estimate is meant to support the Owner with due diligence for the Weston Village BIA Streetscape Improvements and provide information to help inform decisions related to the Master Plan and the annual construction budgets.

We recommend that the owner and/or the design team carefully review the cost estimate report, including line item descriptions, unit price clarifications, exclusions, inclusions and assumptions, contingencies, escalation, and mark-ups. This is to ensure that the design intent is captured within the content of the report.

Please refer to the preamble of our cost report for all exclusions, assumptions, and information pertaining to the estimate.

Requests for modifications of any apparent errors or omissions to this document must be made to A.W. Hooker Associates Ltd. within ten (10) business days of receipt of this estimate. Otherwise, it will be understood that the contents in this estimate have been concurred with and accepted as final version of the cost report.

We trust our work will assist in the decision making process and look forward to our continued involvement in this important project.

Sincerely,  
A.W. Hooker Associates Ltd

Elvan Eryoner, PQS  
Senior Quantity Surveyor

Sincerely,  
A.W. Hooker Associates Ltd

Stew Kyle, PQS, CET  
Partner

Encl. (Class D Estimate (Rev.2) – January 12, 2021)

120179, Weston Village BIA Streetscape Improvements

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## 1. Introduction to the Estimate

### 1.1 Project Description

This project consists of involves an improvement strategy and master plan for the Weston Village BIA. Stretching along Weston Road, north to Church Street and south to Wilby Crescent; west on Lawrence Ave to Hickory Tree Rd and east on Lawrence Ave to South Station, the Weston Village BIA established in 1979 is one of the oldest in Toronto.

### 1.2 Type of Estimate

This Class D Estimate is intended to establish a realistic elemental estimate of the hard construction costs based on the level of design information provided. Detailed quantities have been measured from drawings where possible for the proposed site development. This estimate reflects our opinion as to the fair market value for the hard construction of this project.

The accuracy of the estimate is based on the documentation provided and design stage is intended to be **+/- 25%**. This accuracy is based on the definition for Estimate Classifications (Class D) outlined in the \*Guide to Cost Predictability in Construction prepared by the Joint Federal Government & an Industry Cost Predictability Taskforce. Contingencies are included to offset the accuracy risk, to the extent that the estimated amount represents the current opinion of the likely fair market value at the time of tender.

The intention of the estimate is meant to support the Owner with due diligence for the Weston Village BIA Streetscape Improvements and provide information to help inform decisions related to the Master Plan and the annual construction budgets.

\*Reference: [http://www.cca-acc.com/pdfs/en/CCA/Guide\\_to\\_Cost\\_Predictability.pdf](http://www.cca-acc.com/pdfs/en/CCA/Guide_to_Cost_Predictability.pdf)

## 2. Basis of the Estimate

### 2.1 General Information

From the design information provided, we have measured quantities where possible and applied typical unit rates for each of the specific elements based on the project specifications. Where specific design information has not been provided, unit rates are based on historical cost data for this type of project. In some instances where design information is limited, we have made reasonable assumptions based on our experience with projects of a similar scope and design. Estimates for electrical systems are developed based on historical projects and experience.

Significant changes to the basis of design will impact the estimate value; this is particularly critical where changes are made after the final estimate prior to tender. We recommend that all major design or scope changes be reviewed for their cost, time and constructability impact prior to incorporation in a finalized tender package.

### 2.2 Location Cost Base

The location cost base for this estimate is Toronto, Ontario.

### 2.3 Unit Rates

The unit rates in the preparation of the elemental estimate include labour and material, equipment, and subcontractors overheads and profits. We have assumed for pricing purposes that union or fair wage contractors would perform the work. We have assumed the fair wage policy would be in effect. The unit rates for each of the elements are based on typical mid-range costs for the type of design, construction, and materials proposed.

Unit rates in all estimates combine the material, labour, and equipment components for a single unit cost for ease of presentation. This estimate is not a prediction of low bid. Pricing assumes competitive bidding for every aspect of the work.

## 2.4 Taxes

Harmonized Sales Tax (HST) is excluded from our estimate.

## 2.5 Construction Schedule

The estimate has been prepared on the assumption that the work will be performed within the timelines of a normal construction schedule. The duration of the schedule would be based on the work being performed during regular daytime work hours. We have assumed the structural components of the building would be constructed in predominantly non-winter months. No allowances have been included for premium time and after hours work associated with an accelerated construction schedule.

## 2.6 General Requirements and Fees

The General Requirements for the General Contractor are included as a percentage of the hard construction cost. This estimate of the prime contractor's site overheads includes site supervision and labour, access to the site, site accommodations, site protection, temporary utilities, clean up, equipment, and other miscellaneous project requirements provided by the General Contractor.

The Fee element of the estimate is meant to cover the General Contractor's fee to perform the work. The fee would be based on the competitive nature of the bidding process and the market conditions at the time of tender.

## 2.7 Bonding and Insurance

We have included the median estimated costs for 50% Performance, 50% Labour and Materials, and 10% bid bonds. These are the traditional bonding requirements commonly requested by the owner. The actual final bonding costs will vary depending on the selected contractors' performance history.

The estimate includes an allowance for general liability and builder's risk insurance based on an average cost per \$1,000 of estimated hard construction costs. The actual insurance costs would be subject to the insurance requirements for the project.

## 2.8 Procurement

It was assumed for the preparation of this estimate that the project would be tendered to a prequalified list of bidders with a project specific lump sum contract. Pricing is based on competitive tender results with a minimum of four (preferably six tender submissions) at general contractor and major trade level. Pre-qualification with a restrictive list of contractors or subcontractors may result in a higher tendered cost due to the inherent reduction in competitiveness. Tenders receiving two or less submissions (occasionally three) historically tend to have a much higher risk of an overrun in cost when compared to the budget established in an estimate. Ensuring adequate bonafide bidders is a prerequisite for competitive bidding scenarios, on which the estimate is predicated.

## 2.9 Specifications

As detailed and comprehensive specifications are unavailable, we have assumed that no onerous special requirements will be applicable to this project. It was assumed that all materials and equipment could be substituted with an alternative product to avoid sole-sourcing which results in a non-competitive market condition.

## 2.10 Soft Costs

The estimate includes the soft cost associated Architect/ Engineering Fee. All other soft costs have been excluded from this estimate.

These costs include items traditionally funded by the owner and separate from the hard construction costs which would be applicable to the contractor. The soft costs include items such as consultant fees; disbursements; project management fees; independent inspection and testing; third party commissioning; legal fees; permits and development charges; operational and moving expenses; financing and loan fees; owner supplied furnishings, fixtures, and equipment; land acquisition costs; and Harmonized Sales Tax.

## 3. Contingencies

### 3.1 Design and Pricing Contingency

A design and pricing contingency of **10%** has been included in the estimate as a percentage of the hard construction costs including the general requirements and fees. This contingency is meant to cover design and pricing unknowns in the preparation of this estimate and reflect the incomplete nature of the design information provided at the time the estimate is prepared.

The contingency where included in our estimate is not meant to cover significant additional program space or quality modifications, but rather to provide some flexibility as the design develops. The design contingency typically decreases as the design progresses and more definition and detail is available to refine the basis of the cost estimate. If the owner anticipates significant changes to the basis of design, we recommend additional contingency be retained as a reserve for the scope modifications.

### 3.2 Escalation Contingency

The estimate excludes an allowance for escalation. This allowance, when included, is meant to provide for increases in construction costs due to changes in market conditions between the time of the estimate and the potential construction commencement. For projects with a schedule in excess of 12 months, the contingency is based on a timeframe that takes escalation to the midpoint of the construction phase.

Escalation during construction is included in the unit rates; essentially this allowance is the risk carried by the general contractor and trades with a fixed price made years before the work is completed or carried out for some trades.

### 3.3 Construction Contingency (Post Contract Changes)

The estimate includes a contingency for the construction phase of the project. This contingency is meant to cover the potential cost of post contract changes that may occur after the project is tendered.

This allowance of **10%** is to provide for increases in construction costs due to Change Orders issued during construction.

This contingency excludes any major program or scope requests by the client; these should form part of an overall project management reserve or be reflected in increased funding.



## 4. General Liability

### 4.1 Statement of Probable Costs

A.W. Hooker Associates Ltd. (HOOKER) has no control over the cost of labour and materials, the general contractors or any subcontractors' methods of determining prices, or competitive bidding and market conditions. This opinion of probable cost of construction is based on the experience, qualifications, and best judgment of the professional consultant familiar with the construction industry. HOOKER does not warranty that proposals or actual construction costs will not vary from this or subsequent estimates.

### 4.2 Ongoing Cost Control

A.W. Hooker Associates Ltd. **recommends** that the owner and/or the design team carefully review the cost estimate report, including line item descriptions, unit price clarifications, exclusions, inclusions and assumptions, contingencies, escalation, and mark-ups. This is to ensure that the design intent is captured within the content of the report. This is especially important at early stage estimates which tend to be based on a lesser level of design completion.

If the project is over budget or there are unresolved budget issues, alternative systems or schemes should ideally be evaluated before proceeding with the design phase. We recommend that cost control be implemented throughout the various stages of the design process to ensure the proposed design remains within the overall budget. It is recommended that the final estimate be produced by HOOKER using Bid Documents to determine overall cost changes, which may have occurred since the preparation of this estimate. The final update estimate will address changes and additions to the documents as well as addenda issued during the bidding process. HOOKER cannot reconcile bid results to any estimate not produced from bid documents including all addenda.

## 5. Estimate Scope Clarifications

### 5.1 List of Exclusions

1. Harmonized Sales Tax (HST)
2. Project Soft Costs (as described in item 2.10 above)
3. Furniture, furnishings, and equipment (except as noted in the estimate)
4. Premium time / after hours work
5. Accelerated construction schedule
6. Escalation Contingency Allowance
7. Municipal Permits
8. Abatement and handling of asbestos and other hazardous materials
9. Handling and removal of contaminated/ hazardous soils
10. Premium for construction management or alternate approaches to procurement
11. Sole sourced equipment or systems
12. Soil cells
13. Street Lighting
14. Electrical and Mechanical Services scope of work (except power connection to illuminated signs)
15. Structural scope of work
16. Underground/ above grade Utility scope of work (new/ relocations)
17. Traffic line painting
18. Drinking fountains
19. Trash cans, supplied by CoT
20. Bike share station, supplied by CoT
21. Winter working conditions
22. Illumination (lighting) to "Gateway" sign, the estimate still includes a non-illuminated "Gateway" sign

23. Direct or indirect impacts of any COVID-19, or any other pandemic or epidemic, related events whether known or unknown at the time of the agreement

## 5.2 List of Assumptions

### Architectural / Landscaping:

1. The existing site is relatively flat and the finished floor and site elevations were set to work with the existing grades to avoid major cut and fill.
2. We have assumed 13.0m x 2.0m area for street paint pattern on ground.
3. We have assumed combination of deciduous shrubs, perennials and grasses to Plants in Ground, including planting bed where necessary.
4. We have assumed 2.0m x 4.0m desire line path in 2(two) locations.
5. We have assumed to restore existing low stone wall for 20m long.
6. Curb bump-outs/Sidewalk widening scope includes;
  - a. - removal of existing asphalt
  - b. - removal of existing curbs
  - c. - temporary fence
  - d. - new concrete walkway
  - e. - new concrete curbs
  - f. - asphalt repair on edge of the new curb
  - g. - tactile warning
  - h. - pedestrian crosswalk painting
7. We have assumed one side of Sidewalk widening to Weston Road, which includes;
  - a. - removal of existing asphalt and walkway
  - b. - removal of existing curbs
  - c. - temporary fence
  - d. - new walkway - unit pavers, assumed 2.6m wide
  - e. - new walkway - granite cubes, assumed 1.5m wide
  - f. - bike road, assumed 2.4m wide
  - g. - new curbs
  - h. - asphalt repair on edge of the new curb
  - i. - tactile warning
  - j. - pedestrian crosswalk painting
  - k. - allowance for traffic line painting
  - l. - Planting Allowance (Plants in Ground)
  - m. - allowance for benches, assumed 2NO per 50m
  - n. - allowance for bike rings, assumed 1 NO per 50m

### General:

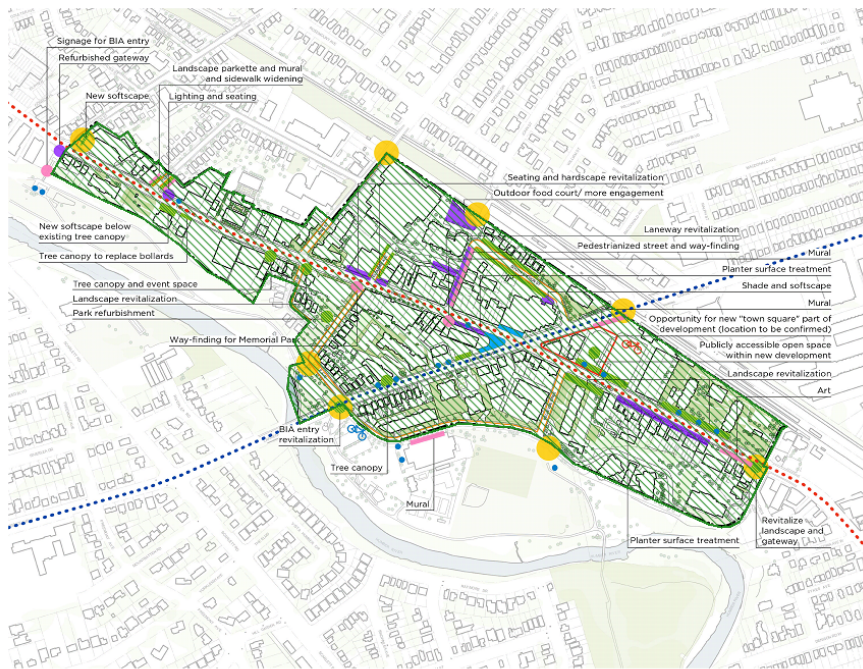
8. Various assumptions have been made based on the design information available and our experience with projects of a similar nature.

## 6. Documentation Received

Drawings and design documentation were provided by PLANT Architect Inc.:

Pages	Documentation	Documentation Received
26 pages	201116_20018 Weston Village BIA_Project Descriptions [fa]	November 16, 2020
1 Excel File	201116_20018_Weston Village BIA_Class D Estimate Structure [fa]	November 16, 2020

## 7. Gross Site Work Area (SWA) – Graphical Representation



**WESTON VILLAGE BIA**  
OVERALL MASTER PLAN  
PHASES 1-3: ALL PROJECTS

N  
SCALE: 1: 5000

## MULTIPLE ESTIMATE SUMMARY WESTON VILLAGE BIA STREETScape IMPROVEMENTS CLASS D ESTIMATE (Rev.2) JANUARY 12, 2021

**A.W. HOOKER**®  
QUANTITY SURVEYORS

	Estimated Total	% of Total
1 Cycle Infrastructure	\$88,655	0.6%
2 Masonic Temple (2040 Weston Road)	\$9,055	0.1%
3 Fern Avenue Bus Stop (2105 Weston Road)	\$75,250	0.5%
4 Murals	\$150,000	1.0%
5 Shoppers Drug Mart and Old TD Bank Forecourt (1975-1995 Weston Road)	\$83,653	0.5%
6 Cruickshank Park Hardscape Refurbishment (NOT IN BIA BOUNDARY)	\$54,400	0.3%
7 Streetscape Improvements (John Street and Peter Kalamaris Lane)	\$52,434	0.3%
8 Hardscape Revitalization (1920 Weston Road)	\$48,294	0.3%
9 Hardscape Revitalization (2100 Lawrence Avenue)	\$47,863	0.3%
10 Streetscape Improvement (1765 Weston Road)	\$100,470	0.6%
11 Streetscape Improvement (1865 Weston Road)	\$60,799	0.4%
12 Streetscape Improvement (Adjacent to 1844 Weston Road)	\$66,836	0.4%
13 Penny Farthing Pole Ornaments	\$194,040	1.2%
14 Hanging Planting Baskets	\$111,250	0.7%
15 Large Temporary Art Piece - Seasonal and Demountable	\$100,000	0.6%
16 Historic Commemoration Walk	\$178,905	1.1%
17 Little Avenue Memorial Park and Wayfinding	\$86,240	0.6%
18 North Gateway (Church Street and Weston Road)	\$221,352	1.4%
19 East Gateway (King Street)	\$68,992	0.4%
20 West Gateway (Lawrence Avenue and Little Avenue) - Partnership w/ Parks	\$68,992	0.4%
21 Southwest Gateway (Bellevue Crescent)	\$241,472	1.5%
22 South Gateway (Wilby Crescent and Weston Road)	\$90,552	0.6%
23 New Parkette (1901 Weston Road)	\$2,659,124	17.0%
24 Parkette Revitalization (Lawrence Avenue)	\$153,507	1.0%
25 Pedestrian Light Pole	\$53,814	0.3%
26 Streetscape Improvement (1754-1836 Weston Road)	\$965,974	6.2%
27 Landscape Parkette (Fern Avenue)	\$180,224	1.2%
28 Streetscape Improvements (Elsmere Avenue)	\$267,775	1.7%
29 Streetscape Improvements (South Station Street)	\$947,864	6.1%
30 Additional Curb Bump-Outs/Sidewalk Widening	\$8,184,176	52.4%
	<b>\$15,611,962</b>	
Imperial Conversion		

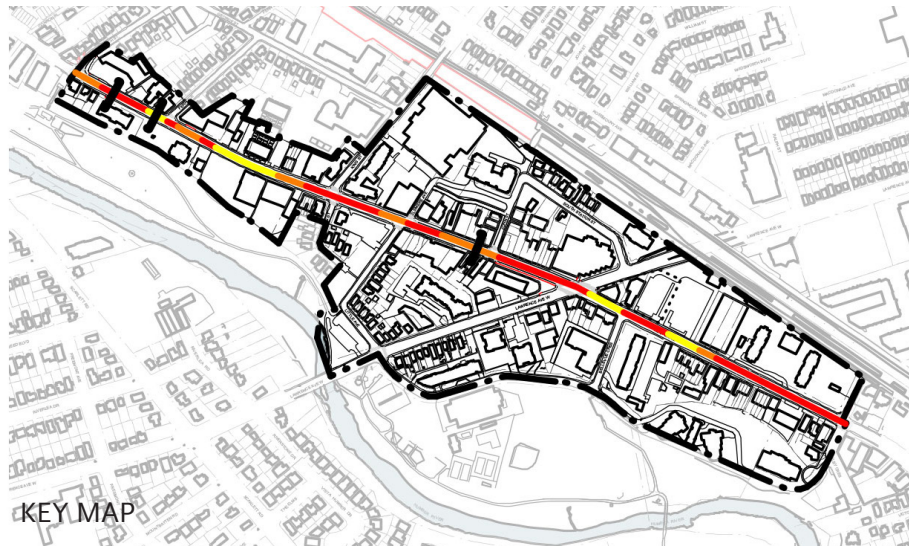


	Quantity	Unit	Unit Cost	Sub-Total	Total Cost	Implementation Comments
<b>Mid Term Works - Interventions Requiring Coordination with City Departments / Expanded Approvals &amp; Partnerships</b>						
						<b>3 – 6 year timeframe</b>
<b>16 Historic Commemoration Walk</b>						
<b>16.1</b> Corten stands for Plaques and Artifacts						
	5	ea.	\$7,500.00	\$37,500.00		
<b>16.2</b> Install Artifacts along Green Passage						
	5	ea.	\$925.00	\$4,625.00		
<b>16.3</b> Planting Allowance to Enhance Green Passage (Plants in Ground)						
	560	m <sup>2</sup>	\$110.00	\$61,600.00		assumed combination of deciduous shrubs, perennials and grasses
						Percentage of net construction cost
						Percentage of net construction cost
						Percentage of sub-total construction cost
						Percentage of sub-total construction cost
<b>17 Little Avenue Memorial Park and Wayfinding</b>						
<b>17.1</b> Corten Steel Sign						
	1	sum	\$20,000.00	\$20,000.00		
<b>17.2</b> Restore Existing (Low) Stone Wall						
	1	sum	\$30,000.00	\$30,000.00		assumed 20m long
						Percentage of net construction cost
						Percentage of net construction cost
						Percentage of sub-total construction cost
						Percentage of sub-total construction cost
<b>18 North Gateway (Church Street and Weston Road)</b>						
						<b>\$221,352.21</b>
<b>18.1</b> Remove Existing Hardscape						
	1	sum	\$8,000.00	\$8,000.00		
<b>18.2</b> Gateway Sign						
	1	ea.	\$40,000.00	\$40,000.00		
<b>18.3</b> Tree Planting						
	5	ea.	\$1,250.00	\$6,250.00		
<b>18.4</b> Tree Education Plaques						
	5	ea.	\$500.00	\$2,500.00		
<b>18.5</b> Planting Allowance (Plants in Ground)						
	1	sum	\$55,000.00	\$55,000.00		assumed combination of deciduous shrubs, perennials and grasses including planting bed
<b>18.6</b> Corten edging around Planting						
	107	m	\$155.00	\$16,585.00		
						Percentage of net construction cost
						Percentage of net construction cost
						Percentage of sub-total construction cost
						Percentage of sub-total construction cost
<b>19 East Gateway (King Street)</b>						
						<b>\$68,992.00</b>
<b>19.1</b> Gateway Sign						
	1	ea.	\$40,000.00	\$40,000.00		
						Percentage of net construction cost
						Percentage of net construction cost
						Percentage of sub-total construction cost
						Percentage of sub-total construction cost
<b>20 West Gateway (Lawrence Avenue and Little Avenue) - Partnership w/ Parks</b>						
						<b>\$68,992.00</b>
<b>20.1</b> New Graphic Inset for Existing Gateway Sign						
	1	ea.	\$10,000.00	\$10,000.00		
<b>20.2</b> Planting Allowance (Plants in Ground)						
	1	sum	\$30,000.00	\$30,000.00		assumed combination of deciduous shrubs, perennials and grasses
						Percentage of net construction cost
						Percentage of net construction cost
						Percentage of sub-total construction cost
						Percentage of sub-total construction cost
<b>21 Southwest Gateway (Bellevue Crescent) - Partnership w/ Parks</b>						
						<b>\$241,472.00</b>
<b>21.1</b> Gateway Sign						
	1	ea.	\$40,000.00	\$40,000.00		
<b>21.2</b> New Guard Rail						
	1	sum	\$50,000.00	\$50,000.00		allowance, detail unknown, assumed benches, hand and softscape
<b>21.3</b> Seating Area						
	1	sum	\$40,000.00	\$40,000.00		assumed combination of deciduous shrubs, perennials and grasses
<b>21.4</b> Planting Allowance (Plants in Ground)						
	1	sum	\$10,000.00	\$10,000.00		
						Percentage of net construction cost
						Percentage of net construction cost
						Percentage of sub-total construction cost
						Percentage of sub-total construction cost
<b>22 South Gateway (Wilby Crescent and Weston Road)</b>						
						<b>\$80,552.00</b>
<b>22.1</b> Gateway Sign/Pole						
	1	ea.	\$40,000.00	\$40,000.00		
<b>22.2</b> Planting Allowance (Plants in Ground)						
	1	sum	\$12,500.00	\$12,500.00		assumed combination of deciduous shrubs, perennials and grasses
						Percentage of net construction cost
						Percentage of net construction cost
						Percentage of sub-total construction cost
						Percentage of sub-total construction cost

	Quantity	Unit	Unit Cost	Sub-Total	Total Cost	Implementation Comments
<b>23 New Parkette (1901 Weston Road)</b>						
						<b>\$2,659,124.16</b>
<b>23.1</b> Replace Existing Benches with New BIA Bench						
	6	ea.	\$4,700.00	\$28,200.00		
<b>23.2</b> Large Self-Watering Planters						
	6	ea.	\$2,000.00	\$12,000.00		
<b>23.3</b> Planting Allowance (Plants in Planters)						
	1	sum	\$1,500.00	\$1,500.00		
<b>23.4</b> Shade Pavilion/Awning						
	1	sum	\$1,500,000.00	\$1,500,000.00		assumed corten gathering structure
						Percentage of net construction cost
						Percentage of net construction cost
						Percentage of sub-total construction cost
						Percentage of sub-total construction cost
<b>24 Parkette Revitalization (Lawrence Avenue)</b>						
						<b>\$153,607.20</b>
<b>24.1</b> Remove Existing Bollards						
	4	ea.	\$150.00	\$600.00		
<b>24.2</b> Replace Unit Paving						
	130	m <sup>2</sup>	\$410.00	\$53,300.00		
<b>24.3</b> Replace Existing Benches with New BIA Bench						
	2	ea.	\$4,700.00	\$9,400.00		
<b>24.4</b> Corten Steel Perforated Tree Surrounds						
	3	ea.	\$6,000.00	\$18,000.00		
<b>24.5</b> Paint Existing Planter Boxes						
	1	sum	\$3,200.00	\$3,200.00		
<b>24.6</b> Large Self-Watering Planters						
	2	ea.	\$2,000.00	\$4,000.00		
<b>24.7</b> Planting Allowance (Plants in New and Existing Planters)						
	1	sum	\$500.00	\$500.00		
						Percentage of net construction cost
						Percentage of net construction cost
						Percentage of sub-total construction cost
						Percentage of sub-total construction cost
<b>25 Pedestrian Light Pole</b>						
						<b>\$63,813.76</b>
<b>25.1</b> Pedestrian Light Pole						
	1	ea.	\$30,000.00	\$30,000.00		As per PLANT Arch. Inc. excluding engineering and hydro fees
						Percentage of net construction cost
						Percentage of net construction cost
						Percentage of sub-total construction cost
						Percentage of sub-total construction cost
<b>Long Term Works - Major Interventions, Park Improvements, Streetscape Work with New Developments / City Capital Projects</b>						
						<b>5 – 10 year timeframe</b>
<b>26 Streetscape Improvement (1754-1836 Weston Road)</b>						
						<b>\$965,974.24</b>
<b>26.1</b> Remove Mosaic Concrete Planters						
	9	ea.	\$250.00	\$2,250.00		
<b>26.2</b> Remove Metal Tree Surrounds						
	9	ea.	\$150.00	\$1,350.00		
<b>26.3</b> Perforated Corten Steel Tree Surrounds						
	17	ea.	\$6,000.00	\$102,000.00		
<b>26.4</b> Softscape Below Trees						
	17	sum	\$350.00	\$5,950.00		assumed combination of deciduous shrubs, perennials and grasses including planting bed
<b>26.5</b> Replace Unit Pavers Along Frontage						
	850	m <sup>2</sup>	\$410.00	\$348,500.00		
<b>26.6</b> Vertical Art Series						
	1	sum	\$100,000.00	\$100,000.00		Allowance provided by PLANT Architect
						Percentage of net construction cost
						Percentage of net construction cost
						Percentage of sub-total construction cost
						Percentage of sub-total construction cost
<b>27 Landscape Parkette (Fern Avenue)</b>						
						<b>\$180,224.35</b>
<b>27.1</b> Tree Planting						
	6	ea.	\$1,250.00	\$7,500.00		
<b>27.2</b> Tree Education Plaques						
	6	ea.	\$500.00	\$3,000.00		
<b>27.3</b> Planting Allowance (Plants in Ground)						
	1	sum	\$48,000.00	\$48,000.00		assumed combination of deciduous shrubs, perennials and grasses including planting bed
<b>27.4</b> Corten edging around Planting						
	98	m	\$155.00	\$15,190.00		
<b>27.5</b> Curb Bump-Outs/Sidewalk Widening						
	1	sum	\$29,800.00	\$29,800.00		
<b>27.6</b> Concrete Removal						
						Included in Item 27.5
						Percentage of net construction cost
						Percentage of net construction cost
						Percentage of sub-total construction cost
						Percentage of sub-total construction cost
<b>28 Streetscape Improvements (Elsmere Avenue)</b>						
						<b>\$287,775.20</b>
<b>28.1</b> Tree Planting						
	7	ea.	\$1,250.00	\$8,750.00		
<b>28.2</b> Tree Education Plaques						
	7	ea.	\$500.00	\$3,500.00		
<b>28.3</b> Corten Steel Perforated Tree Surrounds						
	7	ea.	\$6,000.00	\$42,000.00		
<b>28.4</b> Curb Bump-Outs/Sidewalk Widening						
	1	sum	\$101,000.00	\$101,000.00		
						Percentage of net construction cost
						Percentage of net construction cost
						Percentage of sub-total construction cost
						Percentage of sub-total construction cost

	Quantity	Unit	Unit Cost	Sub-Total	Total Cost	Implementation Comments
<b>29 Streetscape Improvements (South Station Street)</b>					<b>\$947,863.84</b>	
29.1 Tree Planting	25	ea.	\$1,250.00	\$31,250.00		
29.2 Tree Education Plaques	25	ea.	\$500.00	\$12,500.00		
29.3 Corten Steel Perforated Tree Surrounds	25	ea.	\$6,000.00	\$150,000.00		
29.4 Curb Bump-Outs/Sidewalk Widening	1	sum	\$355,800.00	\$355,800.00		
General Requirements	12.00%	%		\$65,946.00		Percentage of net construction cost
General Contractor Overheads + Profit	12.00%	%		\$73,859.52		Percentage of net construction cost
Design Development Contingency	10.00%	%		\$68,935.55		Percentage of sub-total construction cost
Construction Contingency	10.00%	%		\$75,329.11		Percentage of sub-total construction cost
Architect/Engineering Fees	15.00%	%		\$113,743.66		
<b>30 Additional Curb Bump-Outs/Sidewalk Widening</b>					<b>\$8,184,176.00</b>	
30.1 King Street	1	sum	\$492,500.00	\$492,500.00		
30.2 Little Avenue	1	sum	\$352,700.00	\$352,700.00		
30.3 Bellevue Crescent	1	sum	\$174,000.00	\$174,000.00		
30.4 Hickory Tree Road	1	sum	\$515,800.00	\$515,800.00		
30.5 Weston Road (Between Elsmere Avenue and Church Street)	1	sum	\$1,760,000.00	\$1,760,000.00		assumed one side of the Weston Road, walkway and bike route
30.6 Weston Road (Between Wilby Crescent and Lawrence Avenue West)	1	sum	\$1,450,000.00	\$1,450,000.00		assumed one side of the Weston Road, walkway and bike route
General Requirements	12.00%	%		\$569,400.00		Percentage of net construction cost
General Contractor Overheads + Profit	12.00%	%		\$637,728.00		Percentage of net construction cost
Design Development Contingency	10.00%	%		\$595,212.80		Percentage of sub-total construction cost
Construction Contingency	10.00%	%		\$654,734.08		Percentage of sub-total construction cost
Architect/Engineering Fees	15.00%	%		\$982,101.12		
<b>Sub-Total Master Plan</b>					<b>\$15,611,962.25</b>	
	HST (13%)				\$2,029,555.09	
<b>Total Master Plan</b>					<b>\$17,641,517.34</b>	

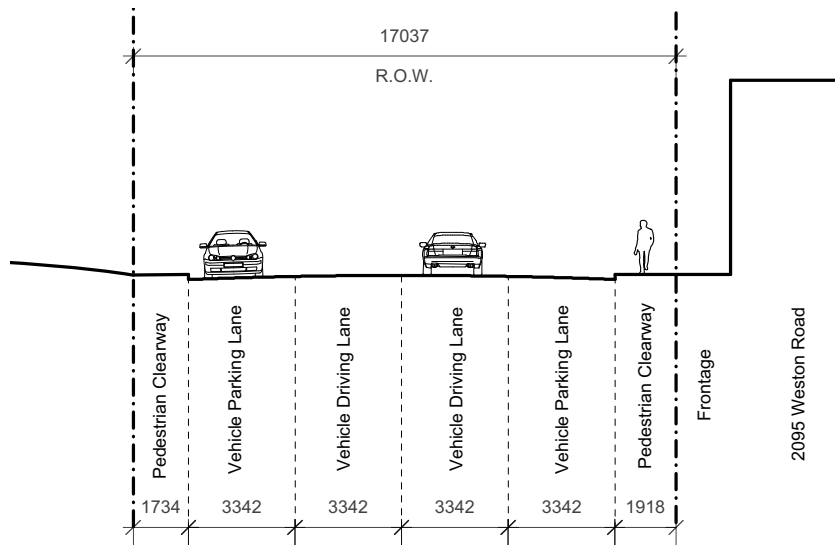
# APPENDIX B: STREET SECTIONS - OPPORTUNITIES



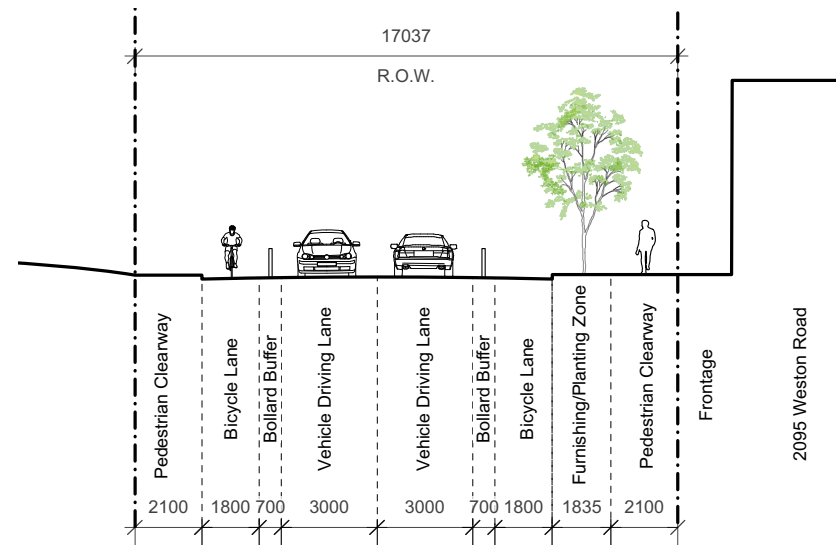
Based on maximizing tree planting in addition to the bikeway, the ROW widths could accommodate the following:

- At the 17 m, ROW width Weston Rd. can accommodate wider sidewalks, street trees and a bikeway in each direction without parking.
- At the 19-21 m, ROW width Weston Rd. can accommodate wider sidewalks, a bikeway in each direction and one lane with shared parking and landscape/vegetated bump-outs.
- At a ROW width over 21 m, Weston Rd. can accommodate wider sidewalks, a bikeway in each direction and two lanes of shared parking and landscape/vegetated bump-outs.

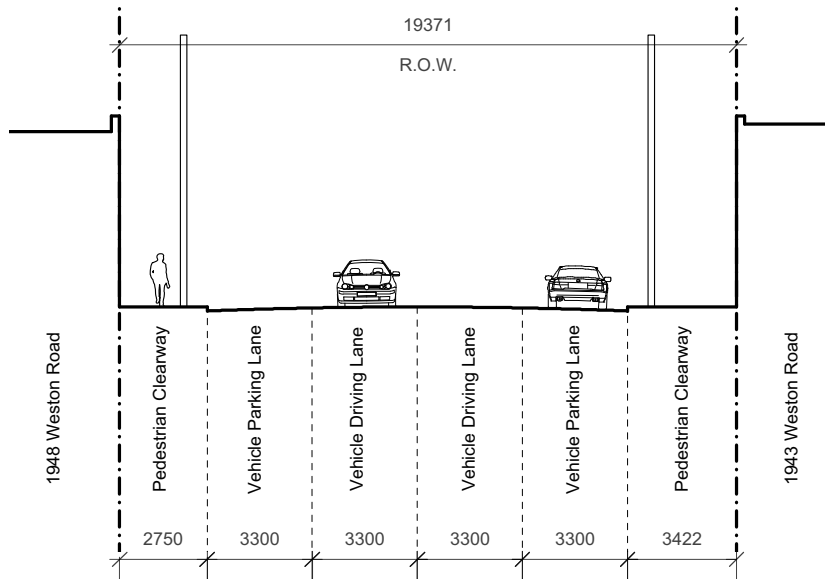
Parking is a concern for any future consideration of bikeways, which would need extensive review with the BIA. Future expectations will take priority for bikeways and infrastructure. The sections shown on the following pages show one possible configuration.



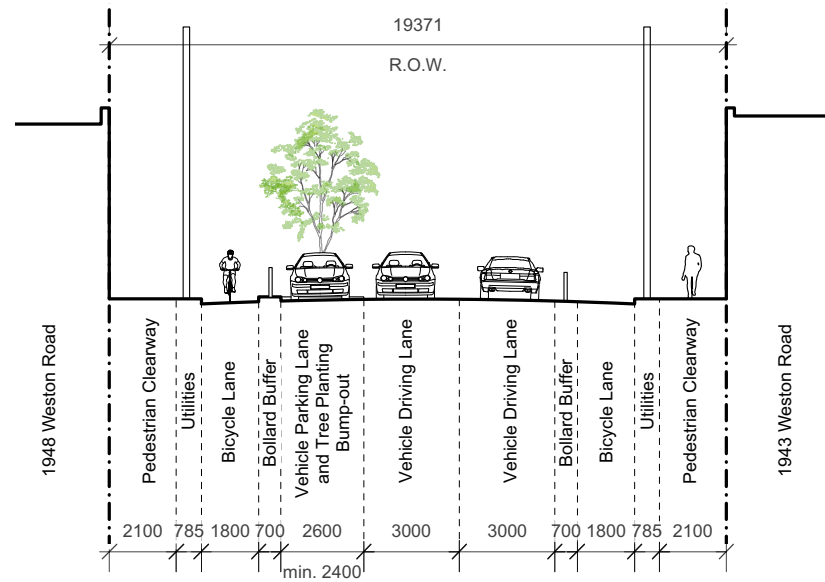
EXISTING WESTON RD. SECTION ROW 17-19m



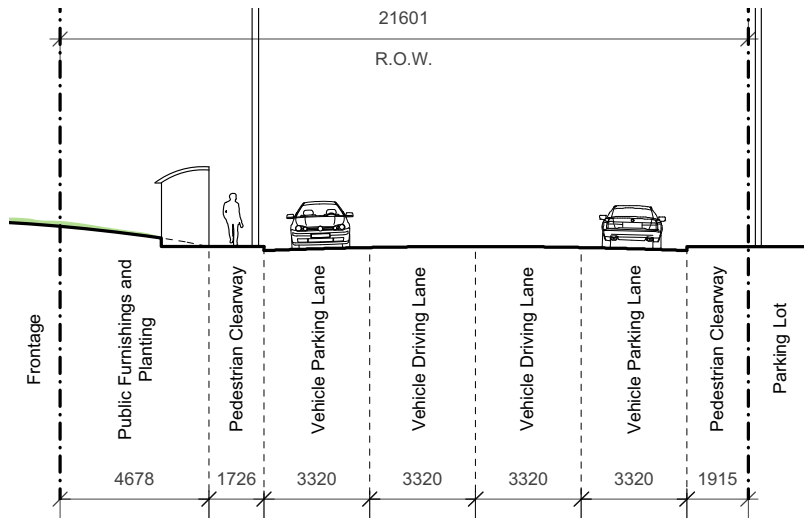
PROPOSED WESTON RD. SECTION ROW 17-19m



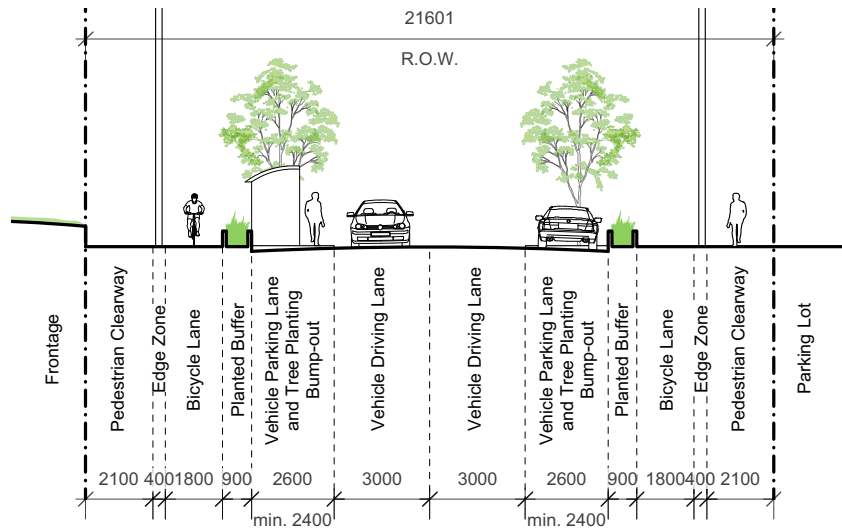
EXISTING WESTON RD. SECTION ROW 19-21m



PROPOSED WESTON RD. SECTION ROW 19-21m



EXISTING WESTON RD. SECTION ROW 21m +



PROPOSED WESTON RD. SECTION ROW 21m +

Sections showing what elements can fit within the varied right of way width along Weston Rd.

# APPENDIX C: NOTES FROM COMMUNITY CONSULTATION







**BENCH**  
BY CLASSIC DISPLAYS



**BIKE RING**  
DESIGNED/MANUFACTURED BY  
ARTIST SCOTT EUNSON



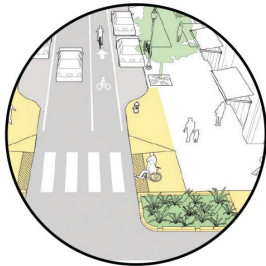
**VERTICAL ART**  
SUSAN SCHMÖGNER



**GATEWAY  
INSPIRATION**  
HISTORICAL SOCIETY



**CURB BUMP-OUT**  
ADDED VEGETATION



**CURB BUMP-OUT**  
ADDED STREET TREES

## PRINCIPLES

### ENHANCE CONNECTIVITY

- Encourage east/west access across the BIA cross streets
- Provide wayfinding gateways for pedestrians, cyclists and cars
  - Activate side streets

### HIGHLIGHT HUMBER RIVER ADJACENCY

- Promote access and wayfinding to and from the river
  - Highlight history and ecology
  - Promote strong green agenda

### AGGREGATE OF SMALL GATHERING NODES

- Distribute seating across BIA in small clusters

### VEGETATION OUT OF HARM'S WAY

- Maximize new tree planting/tree health
- Promote seasonal planting strategies that do not attract litter

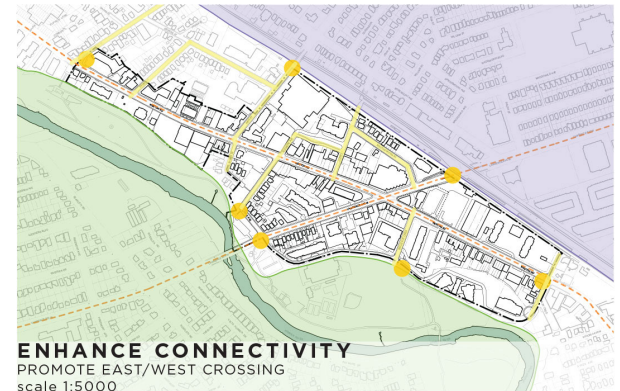
### HISTORIC COMMEMORATION

- Showcase historical narratives and artifacts

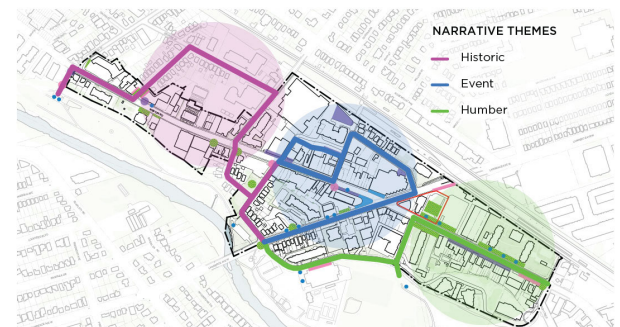
### IT CAME FROM THE LAND AND WESTON MADE IT INTO SOMETHING

- Honour the manufacturing past by highlighting materials and fabrication

Contact: Antonella Nicaso / [Antonella.Nicaso@toronto.ca](mailto:Antonella.Nicaso@toronto.ca) /  
(416) 392 - 1817 / BIA Website: <https://www.westonvillagebia.com>



**ENHANCE CONNECTIVITY**  
PROMOTE EAST/WEST CROSSING  
scale 1:5000



**NARRATIVE THEMES**  
— Historic  
— Event  
— Humber

**NARRATIVE THEMES**  
SECONDARY STREETS AS A PRIMARY PEDESTRIAN AND CYCLE NETWORK  
scale 1:5000



**RIGHT OF WAY WIDTHS**  
— 17 - 19 m  
— 19 - 21 m  
— 21 m +

**WESTON ROAD R.O.W. WIDTHS**  
VARYING R.O.W. WIDTHS ALONG WESTON ROAD  
scale 1:5000

# APPENDIX C: NOTES FROM COMMUNITY CONSULTATION

## SUMMARY

### Top five comments:

- Cycling infrastructure is not sufficient, the BIA should work with City and community partners to further the development of bikeways.
- Incorporate public art that engages and is representative of the community, particularly the youth of Weston.
- Expand the sidewalks, reduce curb radii for safer turns, and add curb bump-outs where possible .
- Add more trees and soft landscaping to the streets.
- Use vertical art elements as way-finding landmarks and sites of commemoration.

### Additional items to consider:

- There has been a busker festival in Weston in past years. <https://www.westonvillagebia.com/buskerfest>.
- The rail path could be an ideal location for north/south cycling infrastructure.
- Consider ways to support the youth of Weston Village, including safe gathering spaces.
- Find a balance between commemorating the history of Weston and acknowledging the community that is there today.
- How can the streetscape support the low-income residents of Weston?

## INVENTORY OF IN-PERSON COMMENTS RECEIVED BY PAI

People were looking forward to beautification, liked what they saw and just wanted to know more, where they can see the final plan, and curious about when it would be implemented.

- Really glad the BIA is doing this.
- Like the idea of vertical artworks.
- Like when the bump outs have plants on them.
- Like the three theme areas.
- Like the idea of bringing more connectivity to the Humber River.
- Q: Is the Railpath being extended?
- Q: How is the historic theme area defined? PAI to look more closely at this.
- Extend the railpath, create parallel bike lanes
- Consider the changing demographics of the neighbourhood
- Athletic facilities: concerns for the community, safety, need better access, we ball for these people.
- Art as backdrop
- Build a stage - the children need a future
- I would love to see cycling infrastructure in Weston “home of the bicycle”. I’ve had many close encounters riding my bike to work from Weston to Mount Dennis.

- Bike lanes and higher safety on roads for bikers.
- Much of the street is either one sided parking or could be so, given the occasional cars
- With construction, it has produced congestion and I don’t see bike lanes creating the same level of congestion based off the amount of traffic I’ve witnessed here
- More bike ways and active transit make for pedestrian friendly cities. Danforth has seen positive results in its business wise too.
- COVID isn’t going away; we are a highly racialized neighbourhood hit hard by COVID and buses are unsafe creating even temporary bike lanes could help relieve crowded buses - provide all transit.



Photograph from public consultation

## INVENTORY OF IN-PERSON COMMENTS RECEIVED BY ANTONELLA

Person 1:

- Large financial gaps between rich (residential to the north) and Weston Rd.'s residential building community.
- Do not want to see further loss of low-income housing along Weston.
- Want to see how streetscaping supports low-income community?

Person 2:

- What is soft landscaping?

Person 3:

- Want bike lanes.

Person 4:

- Pop-up is a great way to inform the community.
- Love the proposals for bump-outs, art installation, Artist's (Scott Eunson) bike ring design, and benches.
- Can any future art installation be kinetic? Ideally, would like to see a different perspective from different angles or movement.
- Low income community deserve art installation as a landmark and a statement that they are not forgotten.

Person 5:

- We need safe gathering spaces for youth.
- There is a lack of state of maintenance at Weston Lion Arena.

- Basketball is the prominent sport that many of the Weston Rd. youth play. The basketball courts at Scarlett are close-by, but poorly maintained. Surface is cracked and uneven. The kids are tripping while playing due to the cracks and existing condition.
- The recreation centre is changing. Improvements were made to the ice hockey facility, but many low-income youth cannot afford to play hockey. The services should reflect the Caribbean, African, Somalin youth that play basketball.
- There is only 1 basketball net at Alexander Park. The youth are territorial and more recreational basketball courts are required in their community to feel safe and to avoid conflict among the youth.
- Weston community does not have its own recreational centre.
- Youth need safe areas to keep them out of trouble. Many have no father and some have no mother. Sport, mainly basketball is their only escape.
- Youth go downtown to play in community tournaments.
- There is a vibrant and raw rap scene in Weston.

Person 6:

- Need separated bike lanes.
- Need Rail-path north bike lanes.

- Poor TTC bus services. The buses are congested. Bike lanes would relieve transit congestion and given COVID, offer safe travel opportunity. We live in a household with immune-compromised parents or grandparents.

Person 7:

- Like to see murals by local artists and about local youth-oriented themes such as "Marky" and "BJ" (residents that were shoot).

Person 8:

- Use art as a landmark. Often, we refer to Popeye as a landmark when providing direction to others.

Person 9:

- More bike lanes.

Person 10:

- Bike lanes that connects Weston Village to Mount Dennis BIA.
- Connect West Toronto Rail Path cycling route up to Weston Rd.
- Separated bike lanes from traffic.
- Connect Bike lanes to Liberty Village. There is a Rail Path cycling route planned in Liberty Village.
- Sad that we missed the opportunity for separated pedestrian/cycling at West Toronto Rail Path.
- What is the feasibility for bike lanes at Jane St.?

# APPENDIX C: NOTES FROM COMMUNITY CONSULTATION

## COMMENT RECEIVED VIA E-MAIL

### OVERALL:

The existing streetscape conditions on Weston Road and Lawrence Avenue West do not support pedestrian or cycling safety or a vibrant public realm. **I strongly support proposals in this Masterplan that reflect City of Toronto *Vision Zero* and *Complete Streets* design guidelines and standards.**

The Weston 2021: Design Initiative and associated [Urban Land Institute \(ULI\) Technical Report](#) also provide a planning framework for streetscape planning initiatives in Weston Village. The proposed Streetscape Masterplan and Principles should be grounded in the visioning expressed in the report. **Ensure this master plan is reflective of the strategies conveyed in the report.**

### 1. STREETScape MASTERPLAN:

**I strongly support public realm interventions and design elements proposed in the Master Plan that encourage a safer and more vibrant public realm, including:**

- Wider sidewalks
- Separated cycling lanes
- Narrower vehicular lanes
- Narrower vehicular turning radii
- Curb bump-outs with landscaping
- Pedestrianization of side streets
- Planting of street trees to form a mature tree canopy
- Low-impact design (LID) stormwater retention features
- Enhanced streetscape furnishings
- Wayfinding signage
- Public realm art

**The scope of this Master Plan should include recommendations for the streetscape area that connects Weston Artscape to Weston Road, specifically the block connection between 22 John Street // the laneway // Elsmere Avenue // John Street to Weston Road.**

- This area of the BIA is a major pedestrian corridor that links neighbourhoods to the commercial district. As the “back door” to Main Street, it needs to be considered in the Masterplan. It has key potential to be revitalized to support BIA public realm goals.

**Develop contemporary design language for proposed street furnishings that is consistent with City of Toronto street furnishing guidelines and unique character of Weston.**

- Sustainable materials
- Fresh, contemporary aesthetic

### 2. PRINCIPLES:

#### Enhance Connectivity

- Prioritizing right-of-way (ROW) structural streetscape changes may be more impactful than aesthetic interventions, such as wayfinding or gateway features. The latter should come secondary to updating the ROW design to meet City of Toronto Vision Zero and Complete Streets design guidelines and standards
- Provide more information to explain “activate side streets”

#### Highlight Humber River Adjacency

- Recommend consulting with Indigenous communities and Toronto and Region Conservation Authority (TRCA) to collaborate on highlighting history and ecology
- Recommend working with City of Toronto Planning and Transportation staff to implement green infrastructure design into streetscape (Low-Impact Design features)

#### Aggregate of Small Gathering Nodes

- Be strategic about seating distribution and types
- Identify areas of the streetscape suited to seating for individuals (ex/ streetscape) versus seating for larger groups (ex/ pedestrian zones: laneways, courtyards, plazas)

#### Vegetation out of Harm’s Way

- Refer to City of Toronto *Complete Streets Guidelines* for recommendations on streetscape planting best practices and design strategies:
  - Streetscape plantings of trees using modular suspended paving systems (ie Silva Cells)
  - Native plant palettes
- Discuss planting maintenance strategies and agreements with City staff and BIA to maintain plants in state of good health

#### Historic Commemoration

- Ensure the realization of this principle is inclusive and meaningful

#### It Came From the Land...

- This idea doesn’t seem like a principle. It seems more like an extension of “Historic Commemoration”

## COMMENT RECEIVED VIA E-MAIL

I am a Mount Dennis resident to the very nearby neighbourhood of Weston Village. I have started the grass-roots neighbourhood group 'Climate Change in Mount Dennis' where we are continuously partnering with Weston Village Organizations to provide mutual aid for the Weston Village community during this time.

I was able to see your presentation of the Weston Village Masterplan where I would love to hear more details!

I admire and recognize the Weston signage with the incorporation of the "Bicycle" as historically, Weston Village is Home of the Bicycle! Although according to the Masterplan bike lanes look like they are proposed along Lawrence Ave W? Is there planning towards bike lanes on Weston Rd., after all as the Home of The Bicycle? Local residents have started a petition, but I would love to hear your insight on this.

To highlight Historic Commemoration, could we incorporate that within the vertical art in addition to modern day realism of this community? As a near by resident, and active member to hearing the voices of Weston Village, It would be amazing to highlight through Vertical Art the faces, lives, and adversities this community currently faces. Even historically, Hurricane Hazel has an

impact on the community that can be displayed through art for remembrance as well. A lot of art in the city is indeed for beautification for the current & future times of communities, but how about allowing the current residents to have their stories and voices expressed through art?

Thirdly, as an advocate of eco-conscious initiatives could we implement a number of initiatives such as Vertical gardens within the outdoor food court, or Roof top greenery bus stop shelters along Weston Rd.

(These initiatives would also provide Green jobs for this community as currently we are two Priority Neighbourhoods [Weston Village & Mount Dennis]) (I will attach photos of initiatives done around the world that also helping the bee population in the Summer as well)

Overall, that is my current feedback from what I can make out of the masterplan presented in the Dropbox! It is exciting to see this community develop as although it is separate from Mount Dennis, it is a nearby community that has more engagement and community present outdoors - where I hope Mount Dennis (further down Weston Rd) will get the same attention in development soon.

I look forward to hearing more from yourself!



*Image included in e-mail comment*



*Photograph from public consultation*



*Photograph from public consultation*

# APPENDIX D: SPECIFICATIONS

AVAILABLE SLAT FINISHES

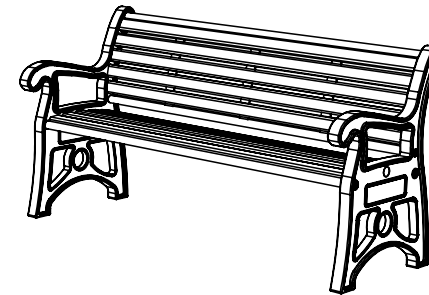
- GREY
- WALNUT
- SAND
- GREEN
- REDWOOD
- OLIVE

WESTON VILLAGE BIA APPROVAL:

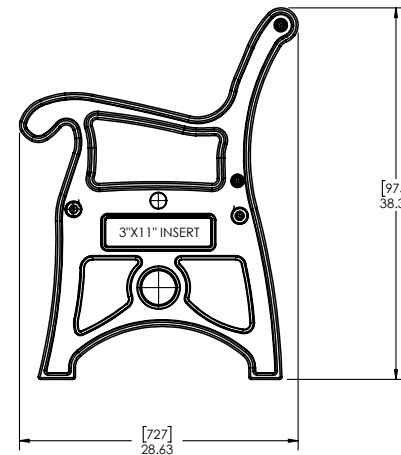
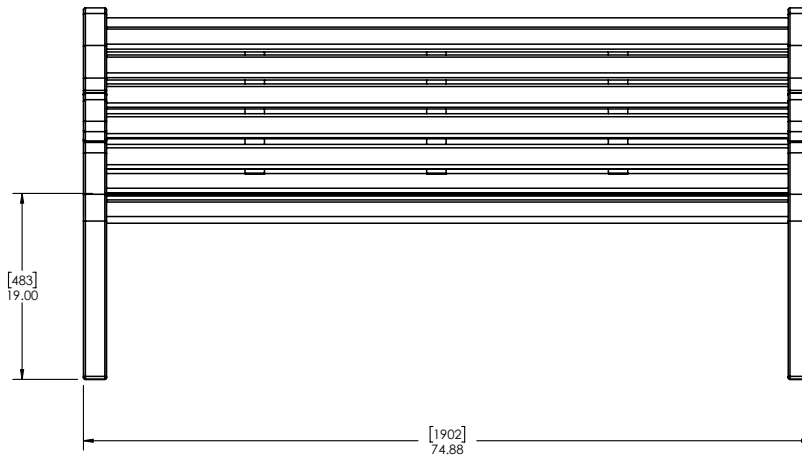
print name: \_\_\_\_\_

signature: \_\_\_\_\_

date: \_\_\_\_\_



ISOMETRIC VIEW




**MATERIALS AND ELEMENTS**

- 100% RECYCLED PLASTIC LUMBER
- ZINC PLATED POWDERCOATED STEEL FRAME
- POWDERCOATED CAST ALUMINUM ENDS
- STAINLESS STEEL HARDWARE

**NOTES:**

1. ELEMENTS POWDERCOAT AVAILABLE IN NUMBER OF STOCK COLOURS
2. INSTALLATION TO BE COMPLETED ACCORDING TO MANUFACTURER'S SPECIFICATIONS
3. ALL DIMENTIONS ARE TRUE AND REFLECT MANUFACTURE'S SPECIFICATIONS

 Classic Displays 5959 AMBLER DR. MISSISSAUGA, ON. TEL: 905-282-8888 CLASSICDISPLAYS.COM PROPRIETARY AND CONFIDENTIAL THE INFORMATION CONTAINED IN THIS DRAWING IS THE SOLE PROPERTY OF CLASSIC DISPLAYS. ANY REPRODUCTION IN PART OR AS A WHOLE WITHOUT THE WRITTEN PERMISSION IS PROHIBITED.	TITLE: HERTIAGE BENCH	
	FOR PRESENTATION PURPOSE ONLY	
	DWG. NO. CD_BEN_HERI	REV A
	SCALE: NTS	SHEET 1 OF 3

*BIA approved bench specification*

# CLASSIC HERITAGE

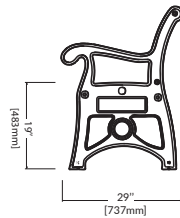
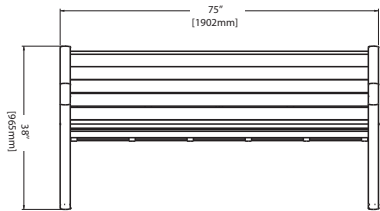


**MATERIALS**

- 100% Recycled Plastic Slats
- Cast Aluminum Ends
- Electrostatically Zinc Plated Steel Harness
- Stainless Steel Hardware

**FINISHES**

- Cast Aluminum Ends And Zinc Treated Steel Components
- Slats Available In Grey, Walnut, Sand, Green, Redwood, Olive And Charcoal
- Glossy Black Powdercoat Finish Resistant To Impact, Moisture, Chipping, Corrosion, And Fading



**TO SPECIFY**

- Slat Colour
- Frame / Ends Colour
- Optional Centre Arm
- Optional Side Name Plates
- Optional Side Lexan Logo

Length	75"	1902mm
Depth	29"	737mm
Height	38"	965mm
Seat Height	19"	483mm
Weight	200 lbs	90 kg

**Optional Centre Arm Configuration**



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*BIA approved bench specification*

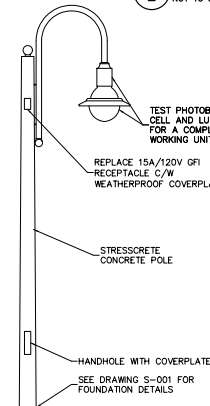
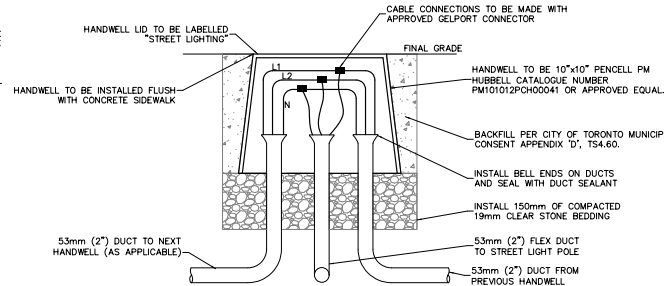
# APPENDIX D: SPECIFICATIONS

## GENERAL CONSTRUCTION NOTES:

- FOR THE PURPOSE OF CONSTRUCTION DRAWINGS, THE FOLLOWING DEFINITIONS SHALL APPLY.
- ALL SITE WORK SHALL BE COMPLETED AS INDICATED ON DRAWINGS.
- GENERAL CONTRACTOR SHALL VISIT THE SITE AND SHALL FAMILIARIZE THEMSELVES WITH ALL CONDITIONS AFFECTING THE PROPOSED WORK AND SHALL MAKE PROVISIONS. GENERAL CONTRACTORS SHALL BE RESPONSIBLE FOR FAMILIARIZING THEMSELVES WITH ALL CONTRACT DOCUMENTS, FIELD CONDITIONS, DIMENSIONS, AND CONFIRMING THAT THE WORK MAY BE ACCOMPLISHED AS SHOWN PRIOR TO COMMENCING WITH CONSTRUCTION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO THE COMMENCEMENT OF WORK.
- ALL MATERIALS FURNISHED AND INSTALLED SHALL BE IN STRICT ACCORDANCE WITH ALL APPLICABLE CODES, REGULATIONS, AND ORDINANCES. GENERAL CONTRACTOR SHALL ISSUE ALL APPROPRIATE NOTICES AND COMPLY WITH ALL LAWS, ORDINANCES, RULES, REGULATIONS, AND LAWFUL ORDERS OF ANY PUBLIC AUTHORITY REGARDING THE PERFORMANCE OF WORK.
- ALL WORK CARRIED OUT SHALL COMPLY WITH ALL APPLICABLE MUNICIPAL AND UTILITY COMPANY SPECIFICATIONS AND LOCAL JURISDICTIONAL CODES, ORDINANCES, AND APPLICABLE REGULATIONS.
- UNLESS NOTED OTHERWISE, THE WORK SHALL INCLUDE FURNISHING MATERIALS, EQUIPMENT, APPURTENANCES, AND LABOUR NECESSARY TO COMPLETE ALL INSTALLATIONS AS INDICATED ON DRAWINGS.
- DRAWINGS ARE NOT TO BE SCALED. THESE PLANS ARE INTENDED TO BE A DIAGRAMMATIC OUTLINE ONLY UNLESS OTHERWISE NOTED. DIMENSIONS SHOWN ARE TO FINISH SURFACES UNLESS OTHERWISE NOTED. SPACING BETWEEN EQUIPMENT IS THE MINIMUM REQUIRED CLEARANCE. THEREFORE, IT IS CRITICAL TO FIELD VERIFY DIMENSIONS, DETAILS ARE INTENDED TO SHOW DESIGN INTENT. MODIFICATIONS MAY BE REQUIRED TO SUIT JOB DIMENSIONS OR CONDITIONS AND SUCH MODIFICATIONS SHALL BE INCLUDED AS PART OF WORK AND PREPARED BY THE ENGINEER PRIOR TO PROCEEDING WITH WORK.
- THE CONTRACTOR SHALL INSTALL ALL EQUIPMENT AND MATERIALS IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS UNLESS SPECIFICALLY STATED OTHERWISE.
- IF THE SPECIFIED EQUIPMENT CANNOT BE INSTALLED AS SHOWN ON THESE DRAWINGS, THE CONTRACTOR SHALL PROPOSE AN ALTERNATIVE INSTALLATION SPACE FOR APPROVAL BY THE ENGINEER PRIOR TO PROCEEDING.
- GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE SAFETY OF WORK AREA, ADJACENT AREAS AND BUILDING OCCUPANTS THAT ARE LIKELY TO BE AFFECTED BY THE WORK UNDER THIS CONTRACT. WORK SHALL CONFORM TO ALL WORK SAFE BUILDING CODE REQUIREMENTS AND LOCAL JURISDICTION.
- GENERAL CONTRACTOR SHALL COORDINATE WORK AND SCHEDULE WORK ACTIVITIES WITH OTHER DISCIPLINES WHERE APPLICABLE.
- CONSTRUCTION SHALL BE DONE IN A WORKMANLIKE MANNER BY COMPETENT EXPERIENCED WORKMAN IN ACCORDANCE WITH APPLICABLE CODES AND THE BEST ACCEPTED PRACTICE.
- NOT USED.
- THE CONTRACTOR SHALL PROTECT EXISTING IMPROVEMENTS, PAVEMENTS, CURBS LANDSCAPING AND STRUCTURES. ANY DAMAGED PART SHALL BE REPAIRED AT CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE OWNER.
- THE CONTRACTOR SHALL CONTACT UTILITY LOCATING SERVICES PRIOR TO THE START OF CONSTRUCTION.
- GENERAL CONTRACTOR SHALL COORDINATE AND MAINTAIN ACCESS FOR ALL TRADES AND CONTRACTORS TO THE SITE AND/OR BUILDING, WHERE APPLICABLE.
- THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR SECURITY OF THE SITE FOR THE DURATION OF CONSTRUCTION UNTIL JOB COMPLETION.
- THE GENERAL CONTRACTOR SHALL MAINTAIN IN GOOD CONDITION ONE COMPLETE SET OF PLANS WITH ALL REVISIONS, ADDENDA, AND CHANGE ORDERS ON THE PREMISES AT ALL TIMES.
- ALL EXISTING ACTIVE SEWER, WATER, GAS, ELECTRIC AND OTHER UTILITIES SHALL BE PROTECTED AT ALL TIMES, AND WHERE REQUIRED FOR PROPER EXECUTION OF WORK, SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER AND EXTREME CAUTION SHOULD BE USED BY THE CONTRACTOR WHEN EXCAVATING OR DRILLING PIERS AROUND OR NEAR UTILITIES.
- CONTRACTOR SHALL MINIMIZE DISTURBANCE TO THE EXISTING SITE DURING CONSTRUCTION. EROSION CONTROL MEASURES, IF REQUIRED DURING CONSTRUCTION, SHALL BE IN CONFORMANCE WITH THE PROVINCIAL AND LOCAL JURISDICTION FOR EROSION AND SEDIMENT CONTROL.
- ALL NECESSARY RUBBISH, STUMPS, DEBRIS, STICKS, STONES AND OTHER REFUSE SHALL BE REMOVED FROM SITE AND DISPOSED OF IN A LAWFUL MANNER.
- ALL BROCHURES, OPERATING AND MAINTENANCE MANUALS, CATALOGS, SHOP DRAWINGS, AND OTHER DOCUMENTS SHALL BE TURNED OVER TO THE GENERAL CONTRACTOR AT UPON COMPLETION OF CONSTRUCTION AND PRIOR TO PAYMENT.
- CONTRACTOR SHALL SUBMIT A COMPLETE SET OF AS-BUILT REDLINES TO THE GENERAL CONTRACTOR UPON COMPLETION OF PROJECT AND PRIOR TO PAYMENT.
- CONTRACTOR SHALL LEAVE PREMISES IN A CLEAN CONDITION.
- NOT USED.
- NOT USED.
- CONTRACTORS SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS AND INSPECTIONS REQUIRED FOR CONSTRUCTION. IF CONTRACTOR CANNOT OBTAIN A PERMIT, THEY MUST NOTIFY THE GENERAL CONTRACTOR IMMEDIATELY.
- CONTRACTOR SHALL REMOVE ALL TRASH AND DEBRIS FROM THE SITE ON A DAILY BASIS.
- INFORMATION SHOWN ON DRAWINGS WAS OBTAINED FROM SITE VISITS AND/OR DRAWINGS PROVIDED BY THE CLIENT. CONTRACTORS SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES PRIOR TO ORDERING MATERIAL, OR PROCEEDING WITH CONSTRUCTION.

## ELECTRICAL NOTES:

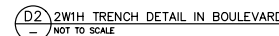
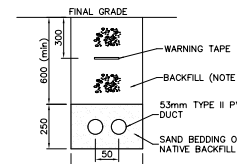
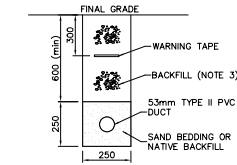
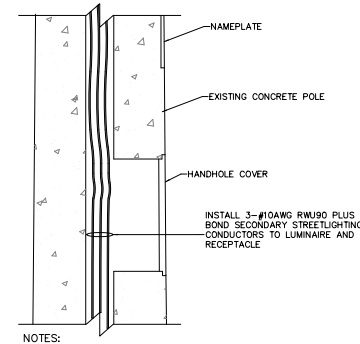
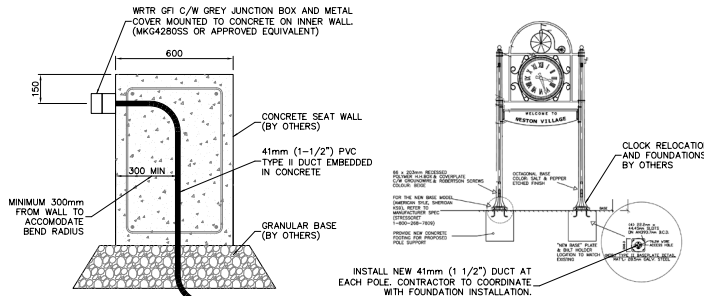
- LOCATION OF OTHER UTILITIES SHOWN ON TRENCH DRAWINGS ARE ASSUMED UNLESS OTHERWISE NOTED. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN ALL UTILITY LOCATES PRIOR TO CONSTRUCTION.
- DEPTH OF COVER TO MEET MCR MINIMUM REQUIREMENTS.
- ALL WORK TO BE DONE IN ACCORDANCE WITH THE LATEST EDITION OF THE ONTARIO ELECTRICAL SAFETY CODE.
- CONTRACTOR TO OBTAIN AND PAY FOR ALL PERMITS AND FEES REQUIRED FOR THE EXECUTION AND INSPECTION OF THE ELECTRICAL WORK. FURNISH A CERTIFICATE OF INSPECTION FROM THE ELECTRICAL SAFETY AUTHORITY UPON COMPLETION OF THE WORK.
- CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE RESULTING FROM OPERATIONS TO EXISTING FACILITIES SUCH AS UNDERGROUND CABLES, HARD SURFACE AREAS, WATERMANS, AND OTHER UTILITIES. CONTRACTOR TO RESTORE, REPLACE, OR REPAIR ANY SUCH DAMAGE TO THE SATISFACTION OF THE ENGINEER.
- RESTORE ALL SURFACES DISTURBED BY ELECTRICAL INSTALLATION TO ORIGINAL CONDITION.
- COORDINATE AND COORDINATE WITH OTHER TRADES ON THE PROJECT.
- USE NEW MATERIALS AND EQUIPMENT UNLESS OTHERWISE STATED.
- ALL SLEEVES, INSERTS, CONCRETE BASES, CURBS, STEEL SUPPORTS, CUTTING, PATCHING, EXCAVATION, TRENCHING, AND/OR BACKFILLING WHERE REQUIRED FOR THE ELECTRICAL INSTALLATION SHALL BE PROVIDED BY THIS CONTRACTOR.
- THE ELECTRICAL CONTRACTOR IS RESPONSIBLE FOR THE SAFETY OF HIS WORKMEN AND EQUIPMENT ON THE PROJECT.
- CONTRACTOR TO PROVIDE ONE SET OF AS-BUILT DRAWINGS FOR THE ELECTRICAL WORK WHERE CHANGES TO THE ORIGINAL DESIGN HAVE OCCURRED.
- FINAL CONNECTIONS OF STREET LIGHT INFRASTRUCTURE TO TORONTO HYDRO SUPPLY BUS ARE TO BE MADE BY TORONTO HYDRO UNLESS OTHERWISE NOTED.



**LUMINAIRE SPECIFICATIONS:**  
CATALOGUE NO.: K211-EGP-III-70(MOG)-HPS-120(TT)-KPL10-BPC  
GLOBE MAT'L: GLASS  
IES CLASSIFIC: TYPE III  
WATTAGE: 70W  
C/W GFI & BA & BPC-II S/F KA18-S  
LIGHT SOURCE: HIGH PRESSURE SODIUM  
LINE VOLTAGE: 120V (TRI-TAP)  
PAINT (SMOOTH BLACK)  
OPTIONS: KPL-10 LEVELING DEVICE  
PHOTOBUITTON CELL

**POLE SPECIFICATIONS:**  
CATALOGUE NO.: P150-BPD-G-E30  
C/W GFI & BA & BPC-II S/F KA18-S  
SECTION: OCTAGONAL  
COLOUR: SALT & PEPPER  
FINISH: ETCHED  
POLE TOP: 5 3/8" FL/FL  
POLE BUTT: 7 1/4" FL/FL  
POLE LENGTH: 15' 0"  
APPROX. WEIGHT: 608 LBS.

**ARM SPECIFICATIONS:**  
CATALOGUE NO.: KA18-S  
MATERIAL: ALUMINUM  
PAINT: SMOOTH BLACK  
COATINGS REQUIRED:  
-B-SILANE FULL LENGTH



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Waterloo, ON N2L 0K4  
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## Notes

- ALL DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.
- FOUNDATION DETAIL IS MODIFIED FROM OPSD 2200.01 TO SUIT EXISTING PEDESTRIAN LUMINAIRES. SEE STANDARD FOR REFERENCE.
- TRENCH IN NON-VEHICULAR AREAS SHALL BE BACKFILLED WITH NATIVE MATERIAL, COMPACTED IN 150mm LAYERS TO 80% SP.
- TRENCH IN VEHICULAR AREAS (ROADWAYS AND DRIVEWAYS) SHALL BE BACKFILLED WITH GRANULAR C/W 100mm LAYERS TO 100% SP. UNSHORABLE FILL IS AN ACCEPTABLE METHOD AS WELL.
- SEE STRESSCRETE DRAWING K01567-1 FOR EXISTING POLE AND LUMINAIRE SPECIFICATIONS.

## Legend

Rev	By	App'd	Date
1	SR	JR	18.11.20
2	SR	JR	19.06.17
3	SR	JR	19.06.17

Permit/Seal

PROFESSIONAL ENGINEER  
133560439008419

Client/Project



**arva** **TORONTO**  
CITY OF TORONTO ASSIGNMENT 9117-17-7338  
WESTON ROAD RESURFACING  
Toronto, ON

Title  
**BIA STREET LIGHT RELOCATIONS  
GENERAL NOTES AND  
INSTALLATION DETAILS**

Project No.	Scale	
1335-60439	AS NOTED	
Drawing No.	Sheet	Revision
E-006	6 of 8	0

Pedestrian lighting pole and clock relocation specifications. Source: the City of Toronto.





**DESERT PLANTERS, a division of Equinox Environmental**

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Winnipeg, Manitoba, Canada

Phone: 204-633-7564

DesertPlanters.com

R2C 5G4

Toll Free: 1-800-665-7487

Sales@DesertPlanters.com

PRODUCT DESCRIPTION

TERRACE 42 - SOLID COLOUR

106.68 cm Dia x 106.68 cm H (42" Dia x 42" H)

Self-watering Desert Planter with 221 litre/58.5 gal water reservoir

10 Year Warranty

HydraFilter Technology

Commercial Grade, Double Wall Rotational Mold, UV Stabilized, Year-Round Use

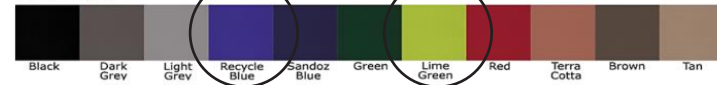
Designed and Manufactured in Canada

**Solid Colour Options:** Black, Brown, Dark Grey, Light Grey, Lime Green, Green, Recycle Blue, Red, Sandoz Blue, Tan, Terra Cotta



**Product Colour Options:**

Solid Colour



Premium Stone



*Self-watering planter recommendation*

**P L A N T**